



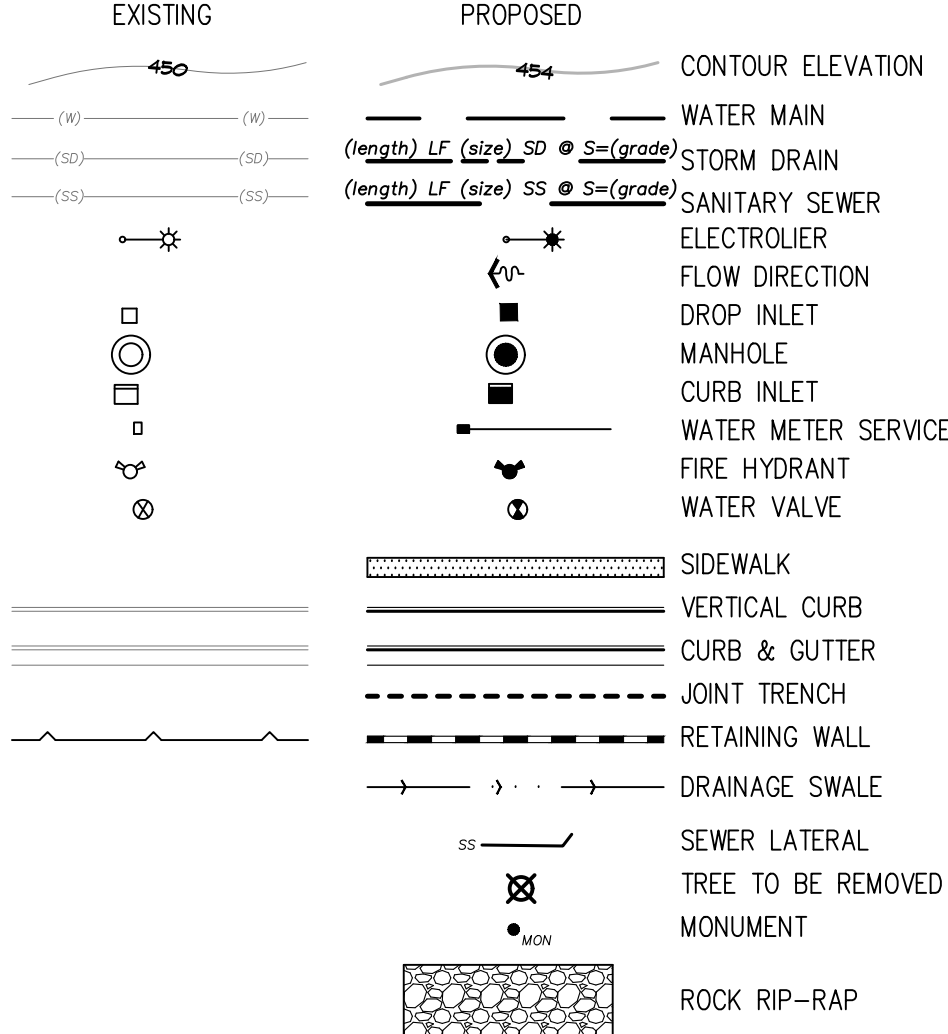


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ABBREVIATIONS

AC	ASPHALT CONCRETE	EP	EDGE OF PAVEMENT	P.S.E.	PUBLIC SERVICE EASEMENT
AB	AGGREGATE BASE	ER	END OF RETURN	P.S.D.E.	PRIVATE STORM DRAIN EASEMENT
AD	AREA DRAIN	ESMT	EASEMENT	P.S.S.E.	PRIVATE SANITARY SEWER EASEMENT
AGG	AGGREGATE	(E)	EXISTING	P.U.E.	PUBLIC UTILITY EASEMENT
BC	BEGINNING OF CURVE	EX.	EXISTING	PVI	POINT OF VERTICAL INTERSECTION
BPD	BACKFLOW PREVENTER DEVICE	FF	FINISH FLOOR	PVC	POLYVINYL CHLORIDE PIPE
BLDG	BUILDING	FG	FINISH GRADE	R	RADIUS
BOC	BACK OF CURB	FB	FIRE HYDRANT	RCP	REINFORCED CONCRETE PIPE
BO	BLOW OFF	FL	FLOWLINE	R/W	RIGHT OF WAY
BOT	BOTTOM	60C	FACE OF CURB	RWL	RAINWATER LEADER
BOW	BACK OF WALK	GM	GAS LINE	S	SLOPE
BW	BOTTOM OF WALL	GB	GRADE BREAK	SD	STORM DRAIN PIPE
BWF	BARBWIRE FENCE	CUY	GUY WIRE FOR POLE	SS	SANITARY SEWER PIPE
CATV	CABLE TELEVISION	GV	GATE VALVE	STM	STORM DRAIN MANHOLE
CB	CATCH BASIN	HDPE	HIGH DENSITY POLYETHYLENE	SS MH	SANITARY SEWER MANHOLE
C&G	CURB & GUTTER	HMA	HOT MIX ASPHALT	SP	SERVICE POLE
CI	CURB INLET	HP	HIGH POINT	STD	STANDARD
CL	CENTERLINE	INV	INVERT OF PIPE	SQ	SQUARE
CMP	CORRUGATED METAL PIPE	IP	IRON PIPE	T	TELEPHONE LINE
CMU	CONCRETE MASONRY UNIT	JP	JOINT POLE	TBM	TEMPORARY BENCHMARK
CO	CLEAN OUT	JT	JOINT TRENCH	TC	TOP OF CURB
CONC.	CONCRETE	LF	LINEAR FEET	TCM	TREATMENT CONTROL MEASURES
CONST	CONSTRUCTION	LP	LOW POINT	TFC	TOP FACE OF CURB
DDCV	DOUBLE DETECTOR CHECK VALVE ASSEMBLY	MAX	MAXIMUM	TC	TOP OF GRATE
DI	DROP INLET	MIN	MINIMUM	TOB	TOP OF BANK
DIP	DUCTILE IRON PIPE	N.I.C.	NOT IN CONTRACT	TOE	TOE OF BANK
DMA	DRAINAGE MANAGEMENT AREA	(N)	NEW	TW	TOP OF WALL
DS	DOWNSPOUT	OHU	OVERHEAD UTILITY	TYP	TYPICAL
DWY	DRIVEWAY	(P)	PROPOSED	W	WATER LINE
E	ELECTRIC LINE	PB	PULL BOX	WM	WATER METER
EC	END OF CURVE	PCC	PORTLAND CONCRETE CEMENT	WV	WATER VALVE
EG	EXISTING GRADE	PL	PROPERTY LINE		
ELEV	ELEVATION	PRC	POINT REVERSE CURVE		

LEGEND



BENCHMARK:

BENCHMARK ID: BM954  
ELEVATION: 340.23 FEET (NAVD88)  
ORGANIZATION: SANTA CLARA VALLEY WATER DISTRICT

DESCRIPTION:

USGS BRASS DISK "A 177"; ALONG UNITED PACIFIC RAILROAD TRACKS; AT 0.2 MILES NORTHWESTERLY FROM TILTON AVENUE; ON TOP OF CONCRETE HEADWALL WHICH IS 1.2 FEET WIDE AND 14 FEET LONG; OVER CULVERT CONSISTING OF TWO 18 INCHES DIAMETER EACH CORRUGATED METAL PIPES; DISK IS ALSO LOCATED AT 5.6 FEET SOUTHEASTERLY FROM THE NORTHWESTERLY END OF WALL. A DATE IS STAMPED ON CONCRETE TOP OF WALL OVER EACH OF TWO 18 INCH DIAMETER PIPES "1932" AND "1938"; DISK IS 79.5 FEET NORTHWESTERLY FROM TRAFFIC SIGNAL STEEL POST; 34.5 FEET NORTHWESTERLY FROM A 24 INCH DIAMETER BLACK WALNUT; 70 FEET SOUTHWESTERLY FROM CENTERLINE MONTEREY ROAD; 28.5 FEET NORTHEASTERLY FROM NORTHEASTERLY RAIL FOR UPRR TRACKS. CITY OF SAN JOSE.

BASIS OF BEARINGS:

THE BEARINGS SHOWN ON THESE PLANS ARE BASED ON THE CENTERLINE OF "VISTA DE LOMAS" WITH A BEARING OF SOUTH 66° 30' 00" EAST AS SHOWN IN THE RECORD OF SURVEY BOOK 321 OF MAPS AT PAGE 23 ON APRIL 19th, 1973. RECORDS OF SANTA CLARA COUNTY.

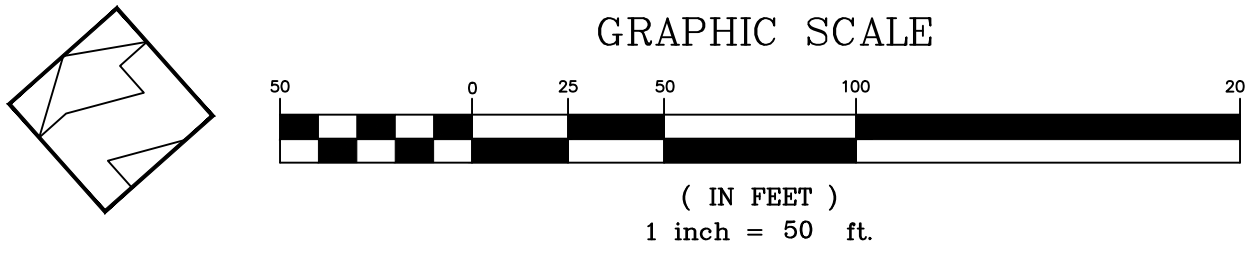
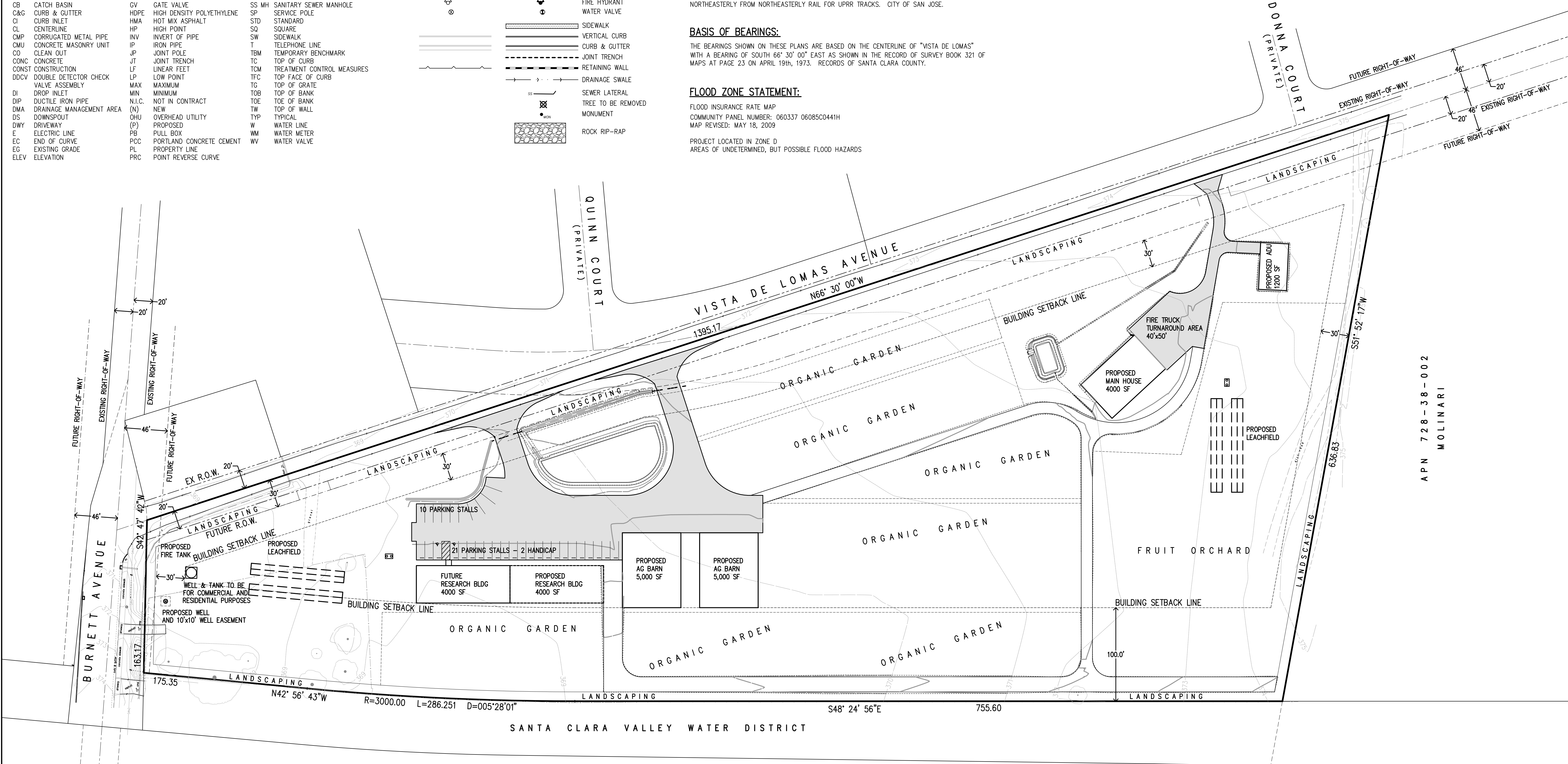
FLOOD\_ZONE STATEMENT:

FLOOD INSURANCE RATE MAP  
COMMUNITY PANEL NUMBER: 060337 06085C0441H  
MAP REVISED: MAY 18, 2009

PROJECT LOCATED IN ZONE D  
AREAS OF UNDETERMINED, BUT POSSIBLE FLOOD HAZARDS

PROJECT SUMMARY

ASSESSOR'S PARCEL NUMBER	728-38-001
SITE AREA	488,745 SQ. FT. / 11.22 ACRES
ZONING	A-40A
LAND USE	AGRICULTURE
PARKING STALLS (1 STALL/350 SF + 1 PER EMPLOYEE)	(PROVIDED) 31 STANDARD STALLS (9'x20') 2 HANDICAP STALLS 33 TOTAL STALLS



PRELIMINARY PLANS  
NOT FOR CONSTRUCTION

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REFER TO ENCROACHMENT AND/OR  
CONSTRUCTION PERMIT AND PLAN  
COVER SHEET FOR SPECIAL  
CONDITIONS AND PERMIT NUMBERS

PLAN #  
SHEET

REVISIONS:		
DATE	DESCRIPTION	BY:

**HANNA-BRUNETTI**  
EST. 1980  
CIVIL ENGINEERS • LAND SURVEYORS  
CONSTRUCTION MANAGERS  
7651 EGGLEBERRY STREET • GILROY • 95020 • CALIFORNIA  
OFFICE (408) 842-2173 • FAX (408) 842-3682  
EMAIL: ENGINEERING@HANNABRUNETTI.COM

DATE: NOVEMBER 2021  
HORIZ. SCALE: 1"=50'  
VERT. SCALE: NONE  
DESIGNED BY: AM  
CHECKED BY: TM  
DRAWN BY: TM

date: 11/20/2021  
Hanna - Brunetti  
Amanda Joy Musy-Verdel  
R.C.E. # 69278



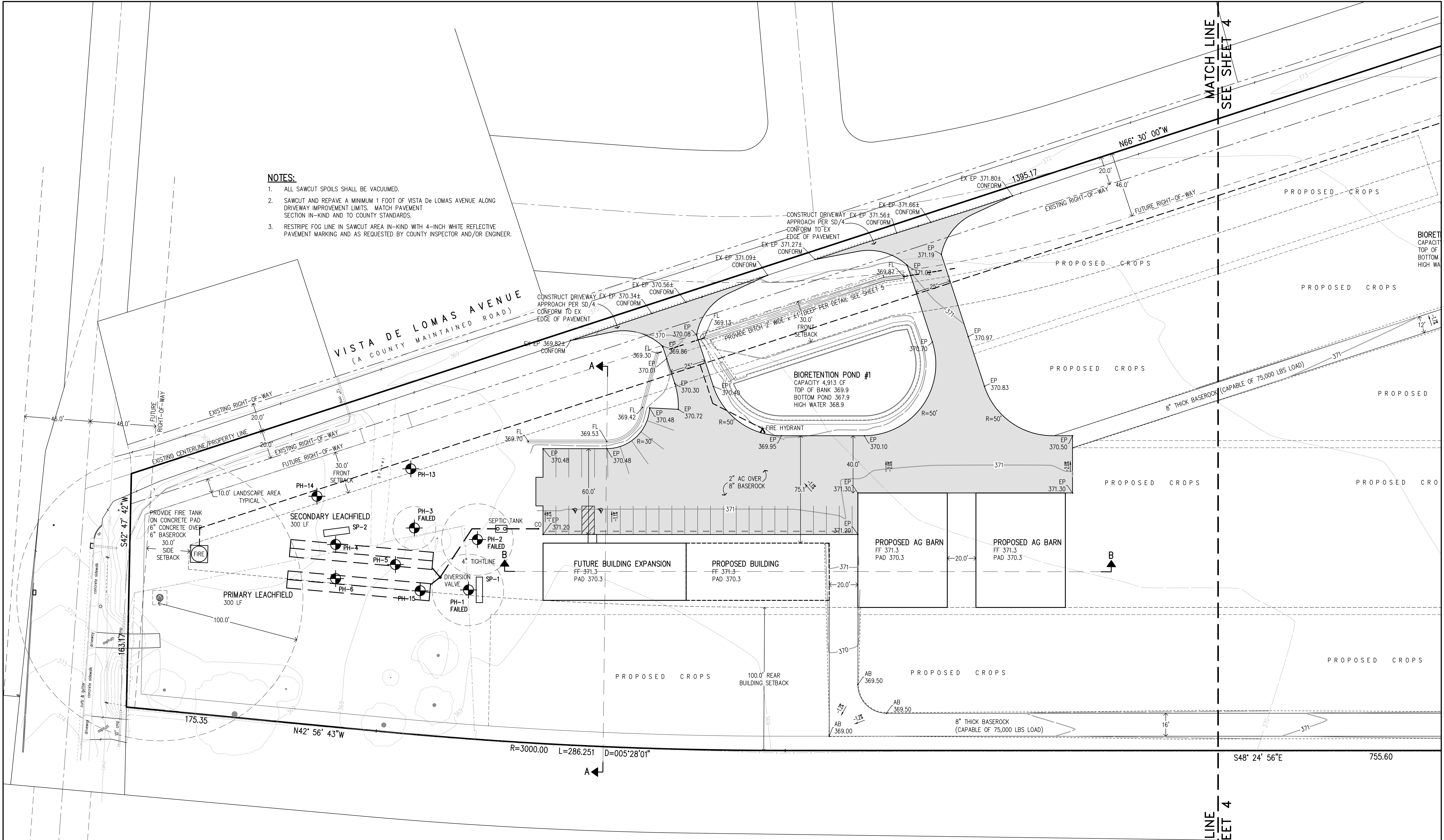
REFERENCES

UNINCORPORATED  
NOVEMBER 2021

Lands of Apex Bait Technologies, Inc. - Burnett & Vista de Lomas Avenue - apn 728-38-001

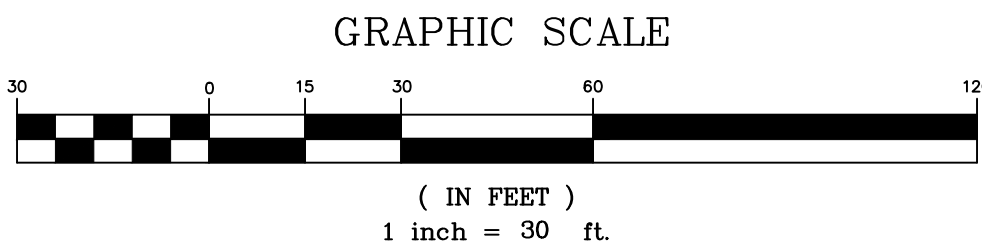
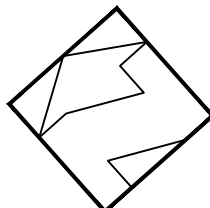
Site Plan

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NOTES:

- ALL SAWCUT SPOILS SHALL BE VACUUMED.
- SAWCUT AND REPAVE A MINIMUM 1 FOOT OF VISTA DE LOMAS AVENUE ALONG DRIVEWAY IMPROVEMENT LIMITS. MATCH PAVEMENT SECTION IN-KIND AND TO COUNTY STANDARDS.
- RESTRIPE FOG LINE IN SAWCUT AREA IN-KIND WITH 4-INCH WHITE REFLECTIVE PAVEMENT MARKING AND AS REQUESTED BY COUNTY INSPECTOR AND/OR ENGINEER.



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Amanda Joy Musy-Verdel  
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REFERENCES

UNINCORPORATED  
NOVEMBER 2021

Preliminary Grading & Drainage Plan

Lands of Apex Bait Technologies, Inc. - Burnett & Vista de Lomas Avenue - apn 728-38-001

SANTA CLARA COUNTY  
CALIFORNIA

SHEET  
3  
OF 8  
JOB NO.  
20058

APPLICANT: APEX BAIT TECHNOLOGIES, INC

ROAD: BURNETT & VISTA DE LOMAS AVENUE

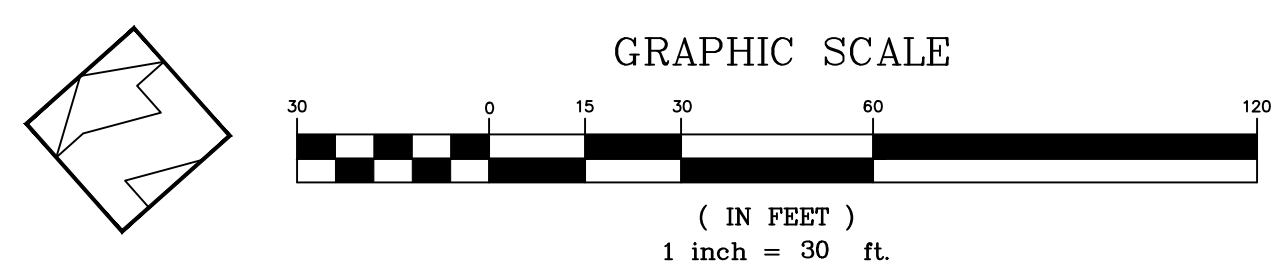
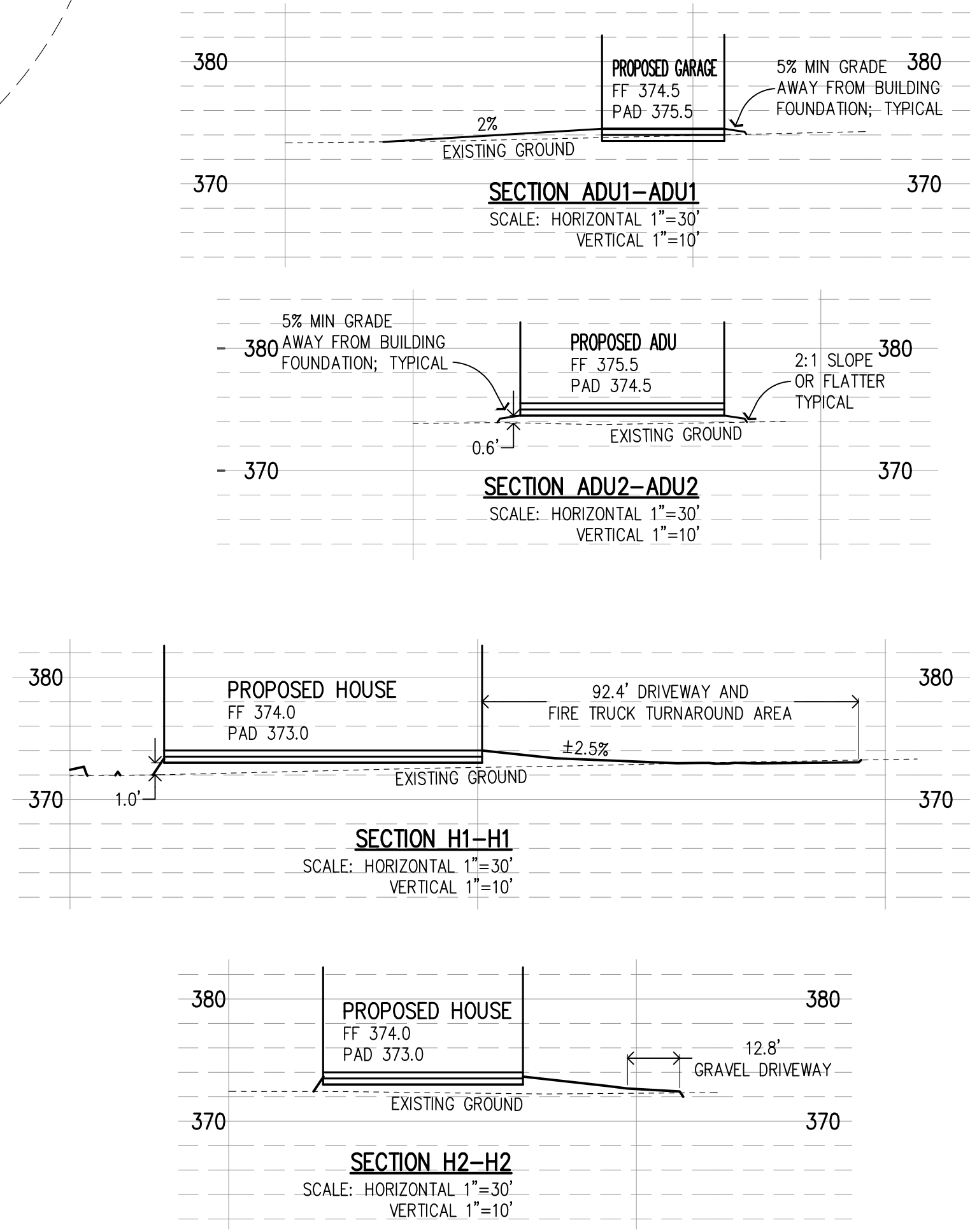
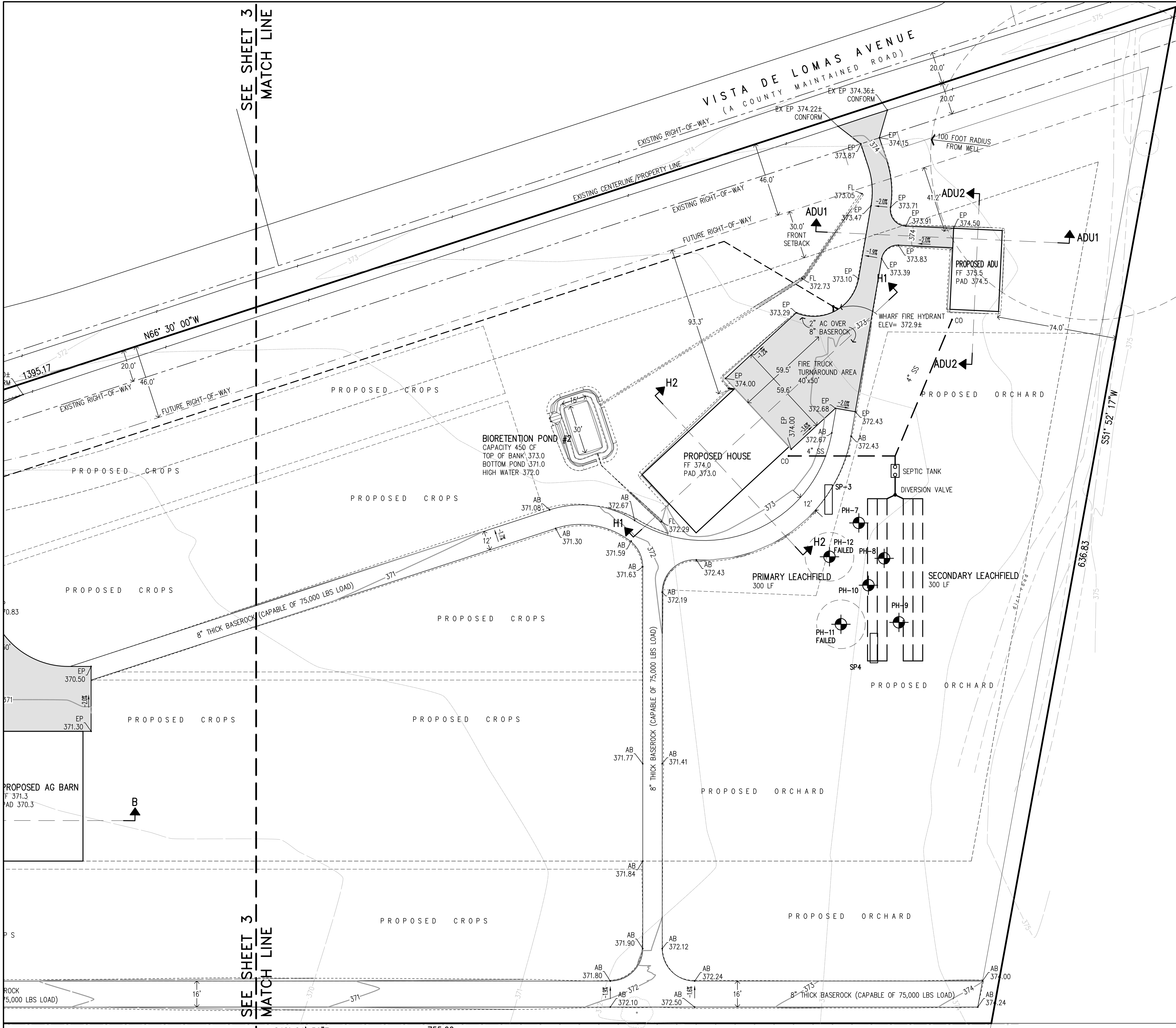
COUNTY FILE NO.:

JOB NO. 20058



PLAN # \_\_\_\_\_ OF \_\_\_\_\_ SHEET

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SHEET  
**4**  
OF 8  
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1. THE LOCATION OF THE BUILDING PADS AND/OR FOUNDATIONS ARE TO BE ESTABLISHED BY A PERSON AUTHORIZED TO PRACTICE LAND SURVEYING. A LETTER SIGNED AND SEALED BY THAT AUTHORIZED PERSON, STATING THAT HE/SHE HAS LOCATED THE BUILDING CORNERS, AND THEIR LOCATIONS CONFORM TO COUNTY BUILDING SETBACK REQUIREMENTS PER THE APPROVED BUILDING PLANS IS REQUIRED TO BE SUBMITTED TO THE COUNTY ENGINEER.
2. THIS PLAN AUTHORIZES THE REMOVAL OF ONLY THOSE TREES WITH TRUNK DIAMETERS GREATER THAN 12 INCHES MEASURED 4.5 FEET ABOVE GROUND WHICH ARE SHOWN TO BE REMOVED. ANY OTHER SUCH TREES ARE NOT TO BE REMOVED UNLESS AN AMENDED PLAN IS APPROVED OR A SEPARATE TREE REMOVAL PERMIT IS OBTAINED FROM THE PLANNING OFFICE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT REMOVAL OF ADDITIONAL TREES HAS BEEN PERMITTED.'
3. NO TREES ARE TO BE REMOVED
4. PRIOR TO GRADING COMPLETION AND RELEASE OF BOND, ALL GRADED AREAS SHALL BE RESEED IN CONFORMANCE WITH THE COUNTY GRADING ORDINANCE TO MINIMIZE THE VISUAL IMPACTS OF THE GRADED SLOPES AND REDUCE THE POTENTIAL FOR EROSION ON THE SUBJECT SITE.
5. BOTH DRAINFIELDS MUST BE STAKED AND STRUNG PRIOR TO APPROVAL OF THE SEPTIC DESIGN TO VERIFY THAT THE PROPOSED SEPTIC DESIGN WILL ACTUALLY FIT INTO THE PROPOSED LEACHFIELD AREA, AND CONFORM TO ALL REQUIRED SETBACKS.
6. IF ARCHAEOLOGICAL RESOURCES OR HUMAN REMAINS ARE DISCOVERED DURING CONSTRUCTION, WORK SHALL BE HALTED WITHIN 50 METERS (150 FEET) OF THE FIND UNTIL IT CAN BE EVALUATED BY A QUALIFIED ARCHAEOLOGIST. IF THE FIND IS DETERMINED TO BE SIGNIFICANT, APPROPRIATE MITIGATION MEASURES SHALL BE FORMULATED AND IMPLEMENTED.
7. NOTIFY SOILS ENGINEER TWO (2) DAYS PRIOR TO COMMENCEMENT OF ANY GRADING WORK TO COORDINATE THE WORK IN THE FIELD.
8. ALL MATERIALS FOR FILL SHOULD BE APPROVED BY THE SOILS ENGINEER BEFORE IT IS BROUGHT TO THE SITE.
9. IN THE EVENT THAT ARCHEOLOGICAL FEATURES SHOULD BE DISCOVERED AT ANY TIME DURING THE GRADING, SCRAPING OR EXCAVATION, ALL WORK SHOULD BE HALTED IN THE VICINITY OF THE FIND AND AN ARCHAEOLOGIST SHOULD BE CONTACTED IMMEDIATELY TO EVALUATE THE DISCOVERED MATERIAL TO ASSESS ITS AREAL EXTENT, CONDITION, AND SCIENTIFIC SIGNIFICANCE. IF THE DISCOVERED MATERIAL IS DEEMED POTENTIALLY SIGNIFICANT, A QUALIFIED ARCHAEOLOGIST SHOULD MONITOR ANY SUBSEQUENT ACTIVITY IN THE PROXIMITY.
10. IN THE EVENT THAT HUMAN SKELETAL REMAINS ARE ENCOUNTERED, THE APPLICANT IS REQUIRED BY COUNTY ORDINANCE NO. B6-18 TO IMMEDIATELY NOTIFY THE COUNTY CORONER. UPON DETERMINATION BY THE COUNTY CORONER THAT THE REMAINS ARE NATIVE AMERICAN, THE CORONER SHALL CONTACT THE CALIFORNIA NATIVE AMERICAN HERITAGE COMMISSION, PURSUANT TO SUBDIVISION (c) OF SECTION 7050.5 OF THE HEALTH AND SAFETY CODE AND THE COUNTY COORDINATOR OF INDIAN AFFAIRS. NO FURTHER DISTURBANCE OF THE SITE MAY BE MADE EXCEPT AS AUTHORIZED BY THE COUNTY CHAPTER. IF ARTIFACTS ARE FOUND ON THE SITE A QUALIFIED ARCHAEOLOGIST SHALL BE CONTACTED ALONG WITH THE COUNTY PLANNING OFFICE. NO FURTHER DISTURBANCE OF THE ARTIFACTS MAY BE MADE EXCEPT AS AUTHORIZED BY THE COUNTY PLANNING OFFICE.
11. THESE PLANS ARE FOR THE WORK DESCRIBED IN THE SCOPE OF WORK ONLY. A SEPARATE PERMIT WILL BE REQUIRED FOR THE SEPTIC LINE CONSTRUCTION.
12. UPPER 6" OF THE SUBGRADE SOIL SHALL BE SCARIFIED, MOISTURE CONDITIONED AND COMPACTED TO A MINIMUM RELATIVE COMPACTION OF 95%.
13. ALL AGGREGATE BASE MATERIAL SHALL BE COMPACTED TO A MINIMUM OF 95% RELATIVE COMPACTION.
14. ROADWAYS DESIGNATED AS NOT COUNTY MAINTAINED ROADS AS SHOWN ON THIS PLAN WILL NOT BE ELIGIBLE FOR COUNTY MAINTENANCE UNTIL THE ROADWAYS ARE IMPROVED (AT NO COST TO THE COUNTY) TO PUBLIC MAINTENANCE ROAD STANDARDS APPROVED BY THE BOARD OF SUPERVISORS AND IN EFFECT AT SUCH TIME THAT THE ROADWAYS ARE CONSIDERED FOR ACCEPTANCE INTO THE COUNTY'S ROAD SYSTEM.
15. AN APPROVED RESIDENTIAL FIRE SPRINKLER SYSTEM COMPLYING WITH FIRE MARSHAL STANDARD CFMO-SP6 IS REQUIRED TO BE INSTALLED THROUGHOUT THE STRUCTURE.
16. ALL NEW ON-SITE UTILITIES, MAINS AND SERVICES SHALL BE PLACED UNDERGROUND AND EXTENDED TO SERVE THE PROPOSED RESIDENCE.
17. A CONSTRUCTION OBSERVATION LETTER FROM THE RESPONSIBLE GEOTECHNICAL ENGINEER AND CERTIFIED ENGINEERING GEOLOGIST DETAILING CONSTRUCTION OBSERVATIONS AND CERTIFYING THAT THE WORK WAS DONE IN ACCORDANCE WITH THE RECOMMENDATIONS IN THE GEOTECHNICAL AND GEOLOGICAL REPORTS SHALL BE SUBMITTED PRIOR TO GRADING COMPLETION AND RELEASE OF BOND.
18. ALL ROOF RUNOFF SHALL BE DIRECTED TO LANDSCAPED OR NATURAL AREAS AWAY FROM BUILDING FOUNDATIONS, TO ALLOW FOR STORM WATER INFILTRATION INTO THE SOIL AND SHEET FLOW.

CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING SURVEY MONUMENTS AND OTHER SURVEY MARKERS DURING CONSTRUCTION. ALL SUCH MONUMENTS OR MARKER'S DESTROYED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONTRACTOR TO VERIFY PRIOR TO CONSTRUCTION OF BUILDING PAD, THE STRUCTURAL SECTION OF FOUNDATION TO DETERMINE BUILDING PAD ELEVATION.



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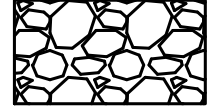
- EROSION CONTROL NOTES:**
- EROSION CONTROL MEASURES SHALL BE EFFECTIVE FOR CONSTRUCTION DURING THE RAINY SEASON; OCTOBER 15 THROUGH APRIL 15.
  - NO STORM WATER RUNOFF SHALL BE ALLOWED TO DRAIN INTO THE EXISTING AND/OR PROPOSED UNDERGROUND STORM SYSTEM UNTIL SUITABLE EROSION CONTROL MEASURES ARE FULLY IMPLEMENTED. NO STORM WATER RUNOFF SHALL BE ALLOWED TO ENTER THE STORM DRAIN SYSTEM THAT IS NOT CLEAR, AND FREE OF SILTS.
  - A FIBER ROLL BARRIER PER "DETAIL SE-5" SHALL BE INSTALL ALONG THE PERIMETER OF THE PROJECT SITE. THE LOCATION OF THE FIBER ROLL ALONG THE PERIMETER SHALL BE ADJUSTED TO ELIMINATE SEDIMENT LADEN RUNOFF FROM LEAVING THE SITE. A FIBER ROLL SHALL ALSO BE REQUIRED AROUND THE PERIMETER OF ANY STOCKPILE OR OTHER SITE OF BARE, LOOSE EARTH.
  - ALL STORM DRAIN MANHOLES, CATCH BASINS, AND/OR DROP INLETS THAT ARE TO ACCEPT STORM WATER SHALL HAVE INLET PROTECTION MEASURES PER DETAIL SE-10. STORM WATER RUNOFF SHALL BE DIRECTED TO THESE INLETS ONLY. STORM DRAIN CATCH BASINS THAT ARE NOT COMPLETE, SHALL BE BLOCKED OFF COMPLETELY.
  - THE NAME, ADDRESS, AND 24 HOUR TELEPHONE NUMBER OF THE PERSON RESPONSIBLE FOR THE IMPLEMENTATION OF THE EROSION CONTROL PLAN SHALL BE PROVIDED TO THE COUNTY.
  - PRIOR TO GRADING, AN ENTRANCE SHALL BE CONSTRUCTED, CONSISTING OF A MINIMUM OF 50 LF OF DRAIN ROCK, 3" IN DIAMETER, PLACED OVER MIRAFI 500X (OR EQUAL) PER DETAIL TC-1. THE ENTRANCE SHALL CONFORM TO "CONSTRUCTION ENTRANCE DETAIL TC-1". THERE SHALL BE ONLY ONE ENTRANCE/EXIT POINT TO THE SITE DURING THE RAINY SEASON. THE LOCATION SHALL BE AS SHOWN ON THESE PLANS, OR AT A LOCATION APPROVED BY THE COUNTY.
  - ALL AREAS OF BARE, TURNED OR DISTURBED EARTH SHALL BE STABILIZED BY USE OF HYDROSEED PER THE TABLE BELOW.  
ALL STOCKPILES, AND/OR BORROW AREAS SHALL BE PROTECTED WITH APPROPRIATE EROSION CONTROL MEASURES SUCH AS A PERIMETER SILT FENCE, AND OTHER METHODS TO PREVENT ANY EROSION OR SILTS MIGRATION. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL DISTURBED AREAS ARE STABILIZED. CHANGES TO THE EROSION CONTROL PLAN SHALL BE MADE TO MEET FIELD CONDITIONS, BUT ONLY WITH THE APPROVAL OF, OR AT THE DIRECTION OF THE COUNTY INSPECTOR. THE STORM DRAIN SYSTEM SHALL MAINTAIN A FORM OF DRAIN INLET PROTECTION UNTIL COUNTY ACCEPTS THE FINAL STREET IMPROVEMENTS. THE DRAIN INLET PROTECTION SHALL BE MAINTAINED, EFFECTIVE AND SUBJECT TO COUNTY INSPECTOR'S APPROVAL.
  - ALL PAVED STREET, AND AREAS ADJACENT TO THE SITE SHALL BE KEPT CLEAR OF EARTH MATERIALS AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO ELIMINATE SEDIMENT LADEN RUNOFF FROM ENTERING THE STORM DRAIN SYSTEM.
  - IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSPECT AND REPAIR ALL EROSION CONTROL FACILITIES AT THE END OF EACH DAY DURING THE RAINY SEASON. ANY DAMAGED STRUCTURAL MEASURES ARE TO BE REPAIRED BY END OF THE DAY. "TRAPPED SEDIMENT IN "SD INLETS" (AND OTHER EROSION CONTROL MEASURES) SHALL BE REMOVED TO MAINTAIN TRAP EFFICIENCY. REMOVED SEDIMENT SHALL BE DISPOSED BY SPREADING ON SITE, WHERE IT WILL NOT MIGRATE.
  - IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PREVENT THE FORMATION OF AIRBORNE DUST NUISANCE AND SHALL BE RESPONSIBLE FOR ANY DAMAGE RESULTING FROM A FAILURE TO DO SO.
  - ALL DRAIN SWALES SHALL BE PER DETAIL EC-9.
  - INCOMPLETE GRADING SHALL NOT BE ALLOWED. CONTRATOR SHALL MAINATIN A DRAIN PATH AS SHOWN ON THIS PLAN. SAID DRAIN PATH SHALL BE MAINTAINED LINED DRAIN SWALES, AND INLET PROTECTION AT A MINIMUM.  
IF PONDING DOES OCCUR ON THE SITE AFTER GRADING, THE WATER MUST BE FREE AND CLEAR OF SEDIMENT PRIOR TO DISCHARGE TO THE STORM DRAIN SYSTEM. THIS REQUIREMENT MAY NECESSITATE THE USE OF NATURAL AND/OR MECHANICAL DESILTING METHODS, SUBJECT TO APPROVAL BY THE COUNTY INSPECTOR.

- EROSION CONTROL NOTES: (CONT)**
- IF THESE EROSION CONTROL MEASURE PROVE INADEQUATE, STRAW MULCH, TACKIFIER, AND ADDITIONAL HYDROSEEDING MAY BE REQUIRED.

HYDROSEED TABLE	
ITEM	LBS/ACRE
COMMON BARLEY	45
ANNUAL RYEGRASS	45
CRIMSON CLOVER	10
FERTILIZER 7-2-3	400
FIBER MULCH	2000
TACKIFIER	100

- ALL GRADING WORK BETWEEN OCTOBER 15th AND APRIL 15th IS AT THE DISCRETION OF THE SANTA CLARA COUNTY BUILDING OFFICIAL.
- PROVIDE SHRUBS AND/OR TREES REQUIRED ON SLOPES GREATER THAN 15 FEET IN VERTICAL HEIGHT.
- THE OWNER/OWNER'S CONTRACTOR, AGENT, AND/OR ENGINEER SHALL INSTALL AND MAINTAIN THROUGHOUT THE DURATION OF CONSTRUCTION AND UNTIL THE ESTABLISHMENT OF PERMANENT STABILIZATION AND SEDIMENT CONTROL WITHIN THE SANTA CLARA COUNTY MAINTAINED ROAD RIGHT OF WAY AND ANY PORTION OF THE SITE WHERE STORM WATER RUN-OFF IS DIRECTLY FLOWING INTO THE SANTA CLARA COUNTY MAINTAINED ROAD RIGHT OF WAY BEST MANAGEMENT PRACTICES (BMP'S) TO PREVENT CONSTRUCTION MATERIALS, EXCAVATED MATERIALS, WASTE MATERIALS, AND SEDIMENT CAUSED BY EROSION FROM CONSTRUCTION ACTIVITIES ENTERING THE STORM DRAIN SYSTEM, WATERWAYS, AND ROADWAY INFRASTRUCTURE. BMP'S SHALL INCLUDE, BUT NOT LIMITED TO, THE FOLLOWING PRACTICES APPLICABLE TO THE PUBLIC ROAD AND EXPRESSWAY FACILITIES:
  - REDUCTION OF POLLUTANTS IN STORM WATER DISCHARGES FROM THE CONSTRUCTION SITE AND THE CONTRACTOR'S MATERIAL AND EQUIPMENT LAYDOWN/STAGING AREAS.
  - PREVENTION OF TRACKING OF MUD, DIRT AND CONSTRUCTION MATERIALS ONTO PUBLIC ROAD RIGHT OF WAY.
  - PREVENTION OF DISCHARGE OF WATER RUNOFF DURING DRY AND WET WEATHER CONDITIONS ONTO PUBLIC ROAD RIGHT OF WAY
- THE OWNER/OWNER'S CONTRACTOR, AGENT, AND/OR ENGINEER SHALL ENSURE THAT ALL TEMPORARY CONSTRUCTION FACILITIES, INCLUDING BUT NOT LIMITED TO CONSTRUCTION MATERIALS, DELIVERIES, HAZARDOUS AND NON-HAZARDOUS MATERIAL STORAGE, EQUIPMENT, TOOLS, PORTABLE TOILETS, CONCRETE WASHOUT, GARBAGE CONTAINERS, LAYDOWN YARDS, SECONDARY CONTAINMENT AREAS, ETC. ARE LOCATED OUTSIDE THE SANTA CLARA COUNTY MAINTAINED ROAD RIGHT OF WAY AND ANY PORTION OF THE SITE WHERE STORM WATER RUN-OFF IS DIRECTLY FLOWING INTO THE SANTA CLARA COUNTY MAINTAINED ROAD RIGHT OF WAY SHALL HAVE SEASONALLY APPROPRIATE BMP'S INSTALLED AND MAINTAINED AT ALL TIMES.

**LEGEND**



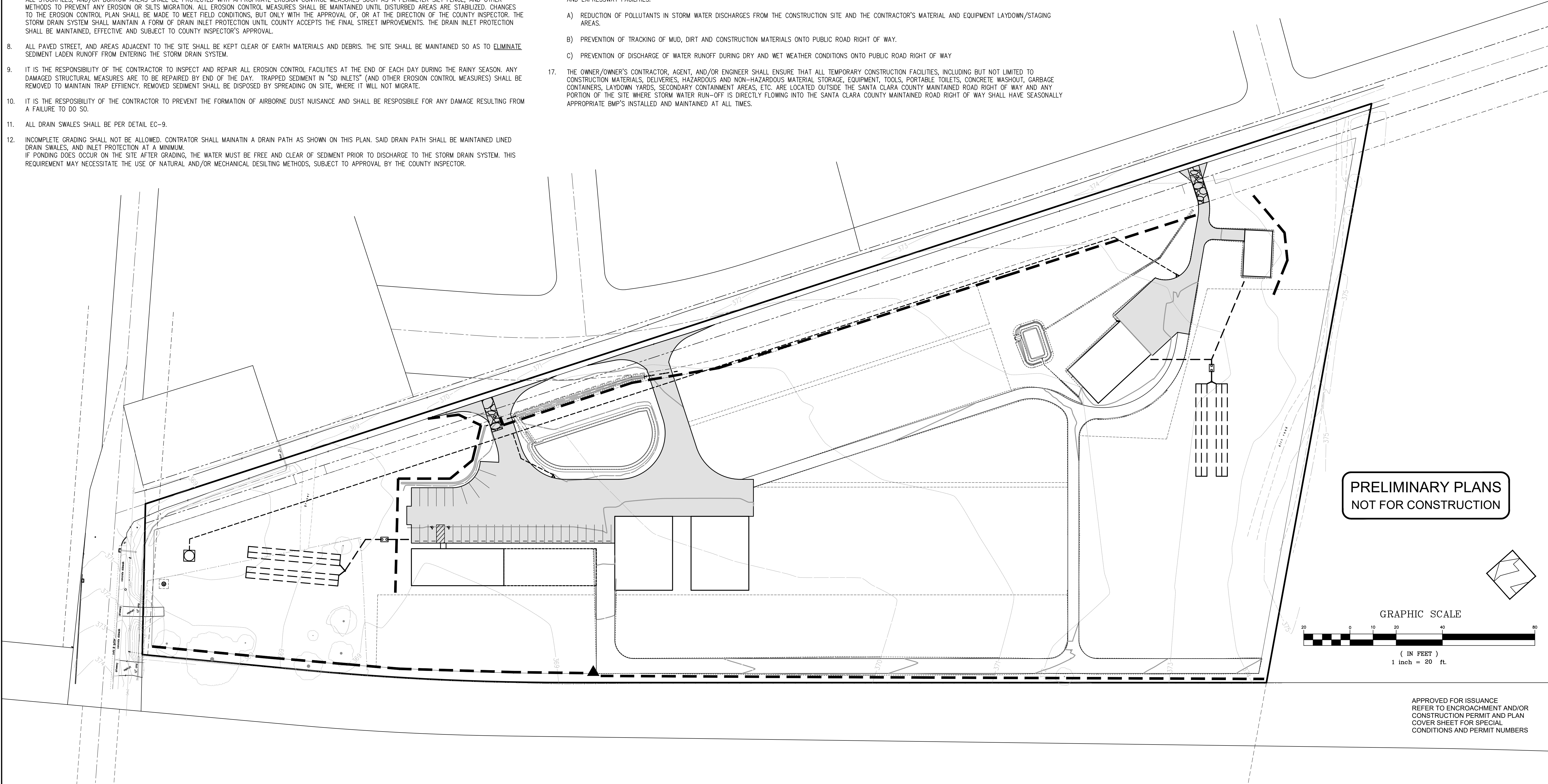
PROVIDE CONSTRUCTION ENTRANCE/EXIT  
PER DETAIL TC-1





PROVIDE FIBER ROLL SLOPE PROTECTION  
PER DETAIL SE-5



PROVIDE STORM DRAIN INLET PROTECTION  
PER DETAIL SE-10



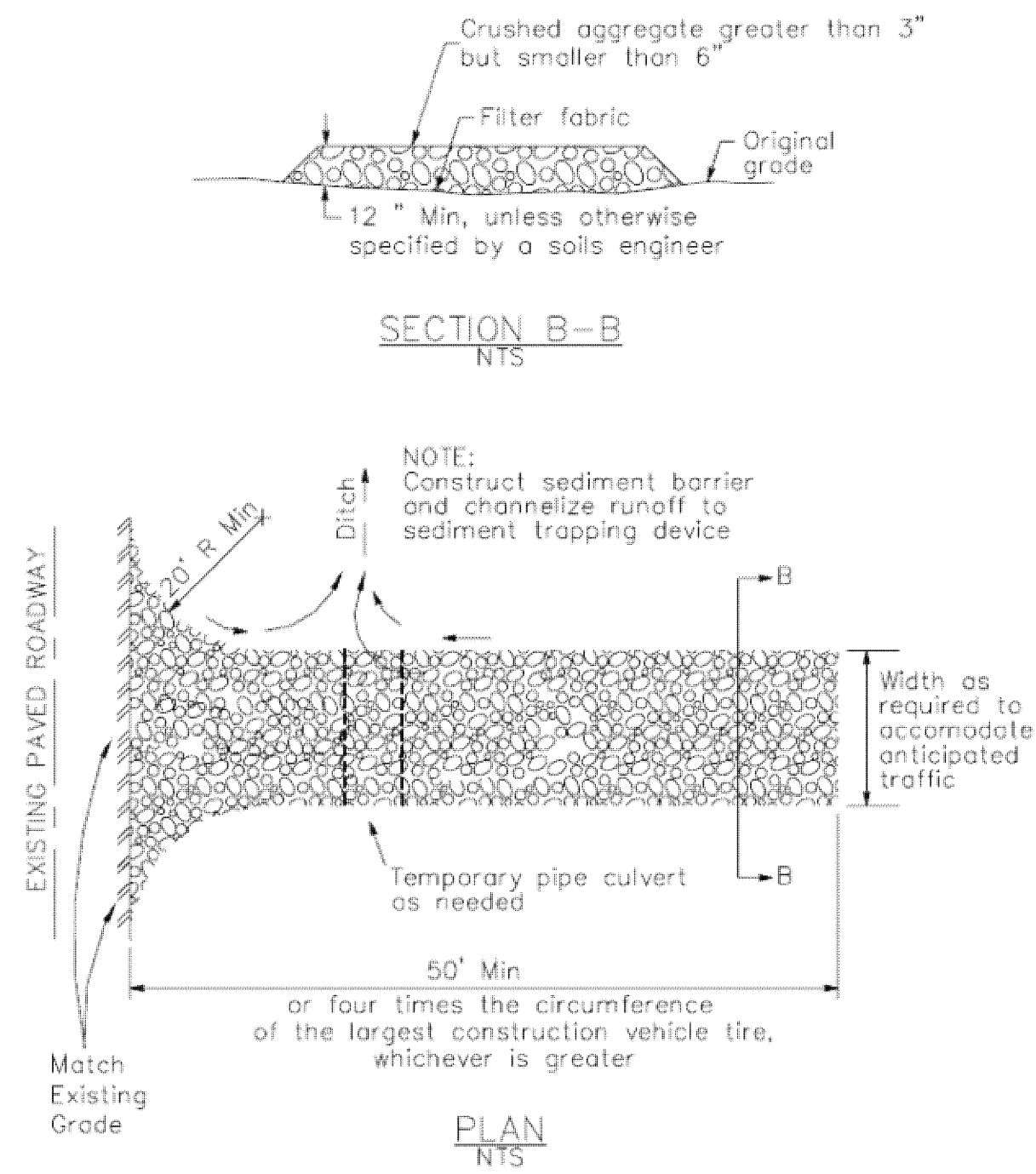
<b>REVISIONS:</b>			 <p><b>HANNA-BRUNETTI</b> EST. 1980 CIVIL ENGINEERS • LAND SURVEYORS CONSTRUCTION MANAGERS 7651 EGGLEBERRY STREET • GILROY • 95020 • CALIFORNIA OFFICE (408) 842-2173 • FAX (408) 842-3662 EMAIL: ENGINEERING@HANNABRUNETTI.COM</p>	DATE: <u>NOVEMBER 2021</u>	 <p>date: <u>          </u> 20 Hanna - Brunetti  Amanda Joy Musy-Verdel R.C.E. # 69278</p>	REFERENCES	<h1>Erosion Control Plan</h1> <p>Lands of Apex Bait Technologies, Inc - Burnett &amp; Vista de Lomas Avenue - apn 728-38-001</p> <p>UNINCORPORATED NOVEMBER 2021</p>	<p>SANTA CLARA COUNTY CALIFORNIA</p>	SHEET	
DATE	DESCRIPTION	BY:		HORIZ. SCALE: <u>1"=50'</u>						6
				VERT. SCALE: <u>NONE</u>						OF 8
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				CHECKED BY: <u>          </u>						20058
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3

**Stabilized Construction Entrance/Exit**

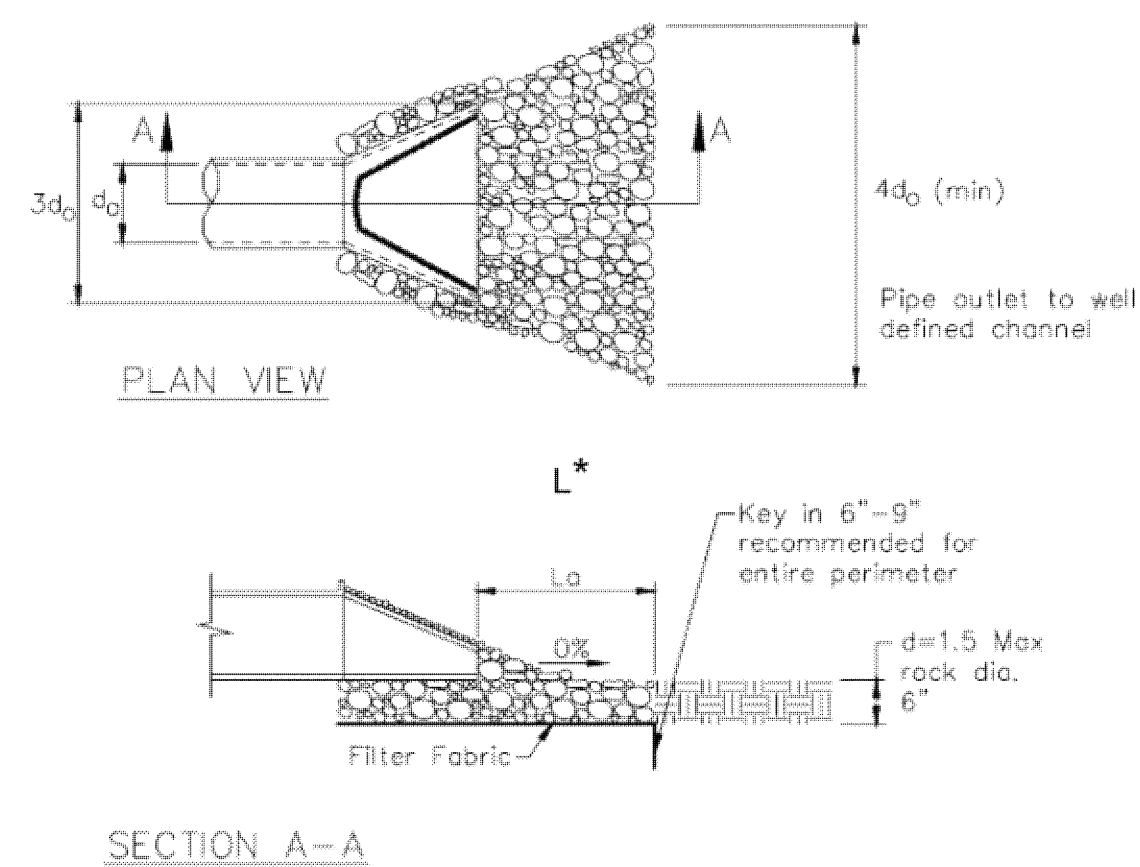
CASQA Detail TC-1



4

**Velocity Dissipation Devices**

CASQA Detail EC-10



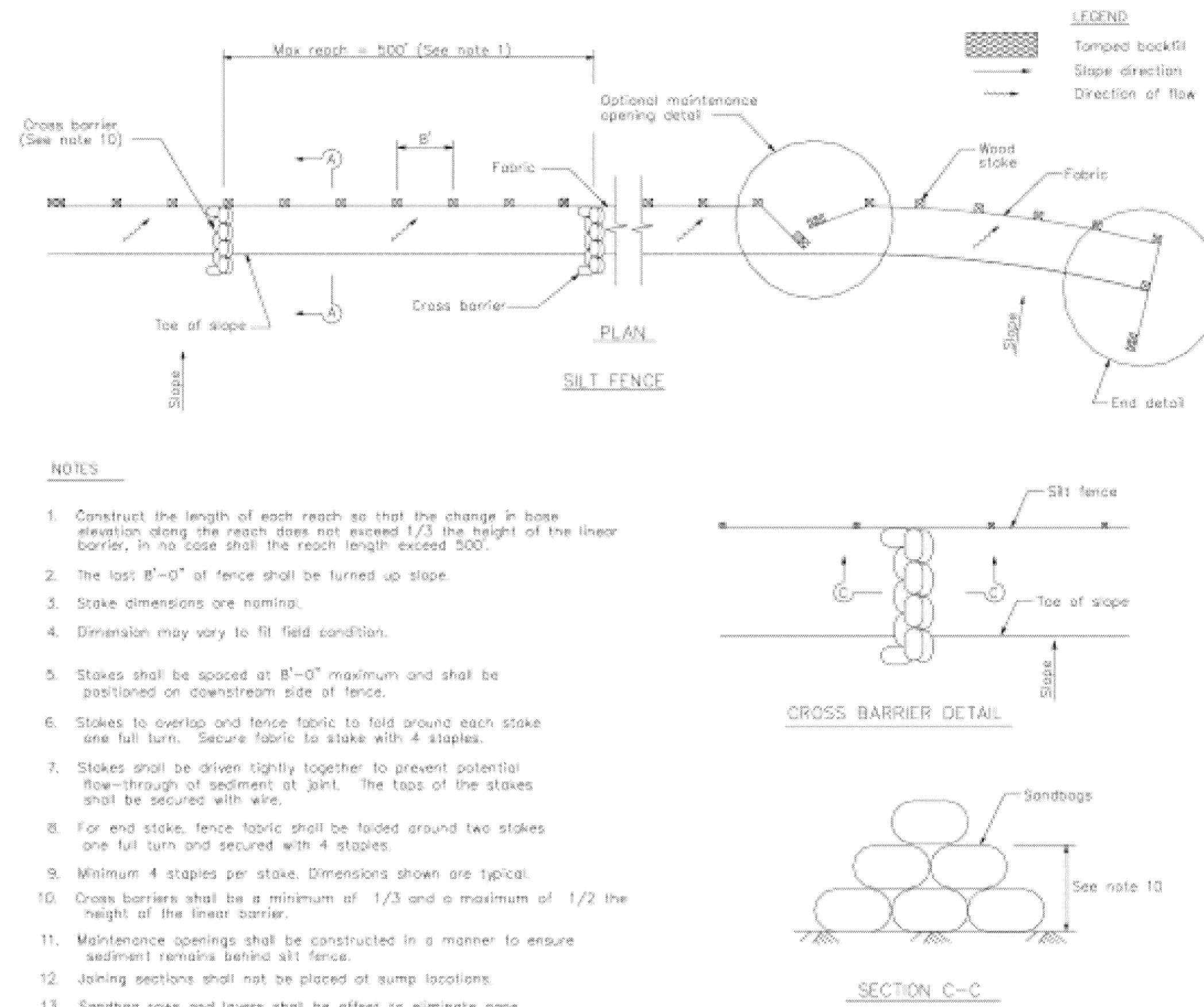
\* Length per ABAG Design Standards

Source for Graphics: California Stormwater BMP Handbook, California Stormwater Quality Association, January 2003.  
Available from [www.cabmphandbooks.com](http://www.cabmphandbooks.com).

1

**Silt Fence**

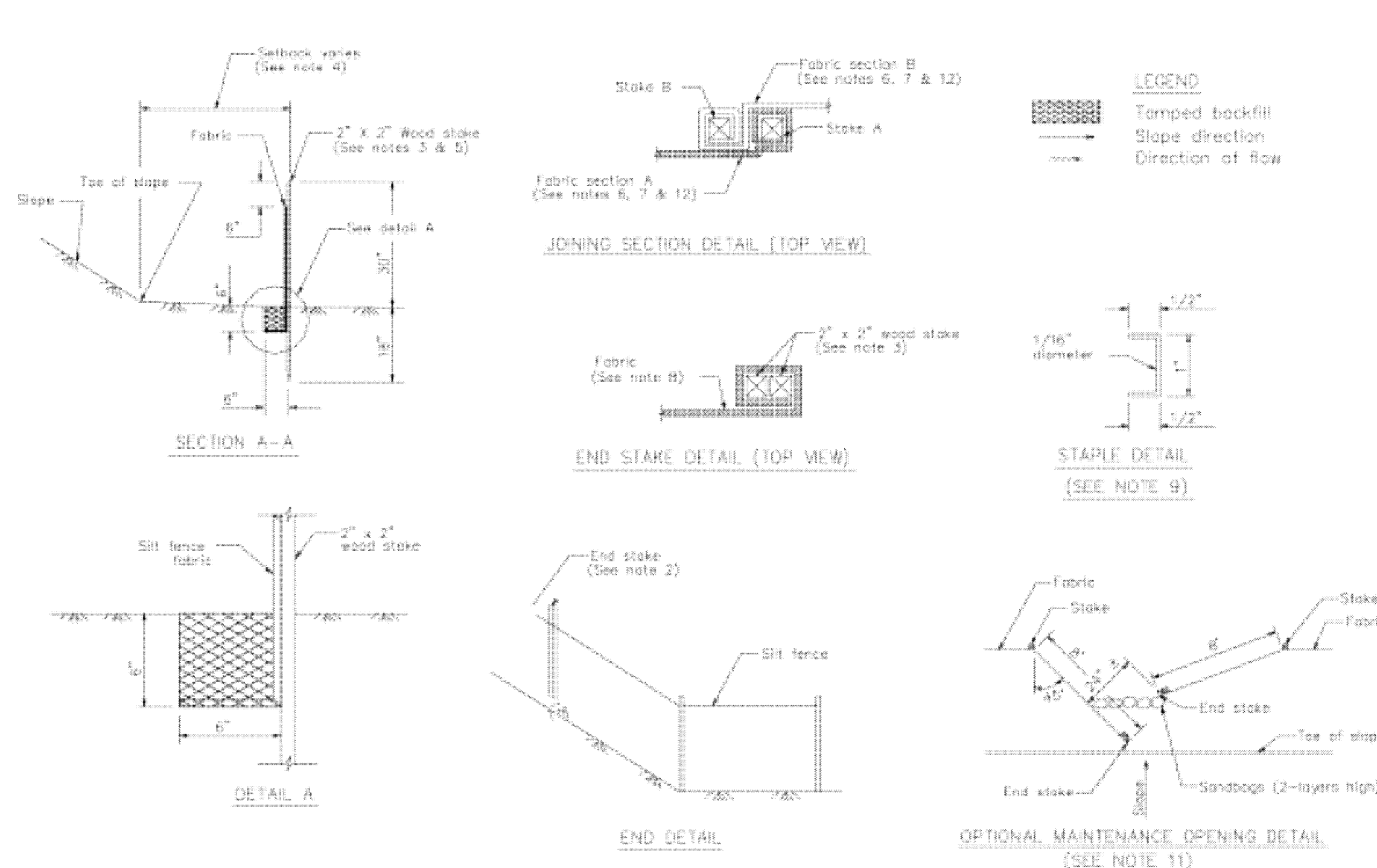
CASQA Detail SE-1



2

**Silt Fence**

CASQA Detail SE-1

**STANDARD BEST MANAGEMENT PRACTICE NOTES**

- Solid and Demolition Waste Management:** Provide designated waste collection areas and containers on site away from streets, gutters, storm drains, and waterways, and arrange for regular disposal. Waste containers must be watertight and covered at all times except when waste is deposited. Refer to Erosion & Sediment Control Field Manual, 4th Edition (page C3) or latest.
- Hazardous Waste Management:** Provide proper handling and disposal of hazardous wastes by a licensed hazardous waste material hauler. Hazardous wastes shall be stored and properly labeled in sealed containers constructed of suitable materials. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-5 to C-6) or latest.
- Spill Prevention and Control:** Provide proper storage areas for liquid and solid materials, including chemicals and hazardous substances, away from streets, gutters, storm drains, and waterways. Spill control materials must be kept on site where readily accessible. Spills must be cleaned up immediately and contaminated soil disposed properly. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-7 to C-8, C-13 to C-14) or latest.
- Vehicle and Construction Equipment Service and Storage:** An area shall be designated for the maintenance, where on-site maintenance is required, and storage of equipment that is protected from stormwater run-on and runoff. Measures shall be provided to capture any waste oils, lubricants, or other potential pollutants and these wastes shall be properly disposed of off site. Fueling and major maintenance/repair, and washing shall be conducted off-site whenever feasible. Refer to Erosion & Sediment Control Field Manual, 4th Edition (page C9) or latest.
- Material Delivery, Handling and Storage:** In general, materials should not be stockpiled on site. Where temporary stockpiles are necessary and approved by the County, they shall be covered with secured plastic sheeting or tarp and located in designated areas near construction entrances and away from drainage paths and waterways. Barriers shall be provided around storage areas where materials are potentially in contact with runoff. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-11 to C-12) or latest.
- Handling and Disposal of Concrete and Cement:** When concrete trucks and equipment are washed on-site, concrete wastewater shall be contained in designated containers or in a temporary lined and watertight pit where wasted concrete can harden for later removal. If possible have concrete contractor remove concrete wash water from site. In no case shall fresh concrete be washed into the road right-of-way. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-15 to C-16) or latest.
- Pavement Construction Management:** Prevent or reduce the discharge of pollutants from paving operations, using measures to prevent run-on and runoff pollution and properly disposing of wastes. Avoid paving in the wet season and reschedule paving when rain is in the forecast. Residue from saw-cutting shall be vacuumed for proper disposal. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-17 to C-18) or latest.
- Contaminated Soil and Water Management:** Inspections to identify contaminated soils should occur prior to construction and at regular intervals during construction. Remediating contaminated soil should occur promptly after identification and be specific to the contaminant identified, which may include hazardous waste removal. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-19 to C-20) or latest.
- Sanitary/Septic Water Management:** Temporary sanitary facilities should be located away from drainage paths, waterways, and traffic areas. Only licensed sanitary and septic waste haulers should be used. Secondary containment should be provided for all sanitary facilities. Refer to Erosion & Sediment Control Field Manual, 4th Edition (page C-21) or latest.
- Inspection & Maintenance:** Areas of material and equipment storage sites and temporary sanitary facilities must be inspected weekly. Problem areas shall be identified and appropriate additional and/or alternative control measures implemented immediately, within 24 hours of the problem being identified.

**STANDARD EROSION CONTROL NOTES**

- Sediment Control Management:**

**Tracking Prevention & Clean Up:** Activities shall be organized and measures taken as needed to prevent or minimize tracking of soil onto the public street system. A gravel or proprietary device construction entrance/exit is required for all sites. Clean up of tracked material shall be provided by means of a street sweeper prior to an approaching rain event, or at least once at the end of each workday that material is tracked, or, more frequently as determined by the County Inspector. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages B-31 to B-33) or latest.

**Storm Drain Inlet and Catch Basin Inlet Protection:** All inlets within the vicinity of the project and within the project limits shall be protected with gravel bags placed around inlets or other inlet protection. At locations where exposed soils are present, staked fiber roles or staked silt fences can be used. Inlet filters are not allowed due to clogging and subsequent flooding. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages B-49 to B-51) or latest.

**Storm Water Runoff:** No storm water runoff shall be allowed to drain in to the existing and/or proposed underground storm drain system or other above ground watercourses until appropriate erosion control measures are fully installed.

**Dust Control:** The contractor shall provide dust control in graded areas as required by providing wet suppression or chemical stabilization of exposed soils, providing for rapid clean up of sediments deposited on paved roads, furnishing construction road entrances and vehicle wash down areas, and limiting the amount of areas disturbed by clearing and earth moving operations by scheduling these activities in phases.

**Stockpiling:** Excavated soils shall not be placed in streets or on paved areas. Borrow and temporary stockpiles shall be protected with appropriate erosion control measures (tarps, straw bales, silt fences, etc.) to ensure silt does not leave the site or enter the storm drain system or neighboring watercourse.
- Erosion Control:** During the rainy season, all disturbed areas must include an effective combination of erosion and sediment control. It is required that temporary erosion control measures are applied to all disturbed soil areas prior to a rain event. During the non-rainy season, erosion control measures must be applied sufficient to control wind erosion at the site.

- Inspection & Maintenance:** Disturbed areas of the Project's site, locations where vehicles enter or exit the site, and all erosion and sediment controls that are identified as part of the Erosion Control Plans must be inspected by the Contractor before, during, and after storm events, and at least weekly during seasonal wet periods. Problem areas shall be identified and appropriate additional and/or alternative control measures implemented immediately, within 24 hours of the problem being identified.
- Project Completion:** Prior to project completion and signoff by the County Inspector, all disturbed areas shall be reseeded, planted, or landscaped to minimize the potential for erosion on the subject site.
- It shall be the Owner's/Contractor's responsibility to maintain control of the entire construction operation and to keep the entire site in compliance with the erosion control plan.
- Erosion and sediment control best management practices shall be operable year round or until vegetation is fully established on landscaped surfaces.

GRADING &amp; DRAINAGE PLAN

FOR THE

DEVELOPMENT OF THE LANDS

FOR APEX BAIT TECHNOLOGIES, INC.

BURNETT &amp; VISTA DE LOMAS AVENUE

A PARCEL OF LAND BEING A PART OF THE ORIGINAL J.M. TONEY TRACT

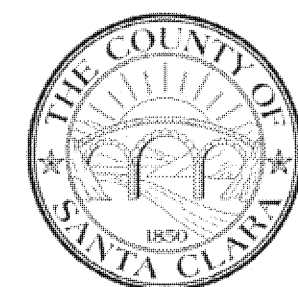
FORMERLY OWNED BY JAMES A. CLAYTON &amp; COMPANY

IN THE RANCHO LA LAGUNA SECA

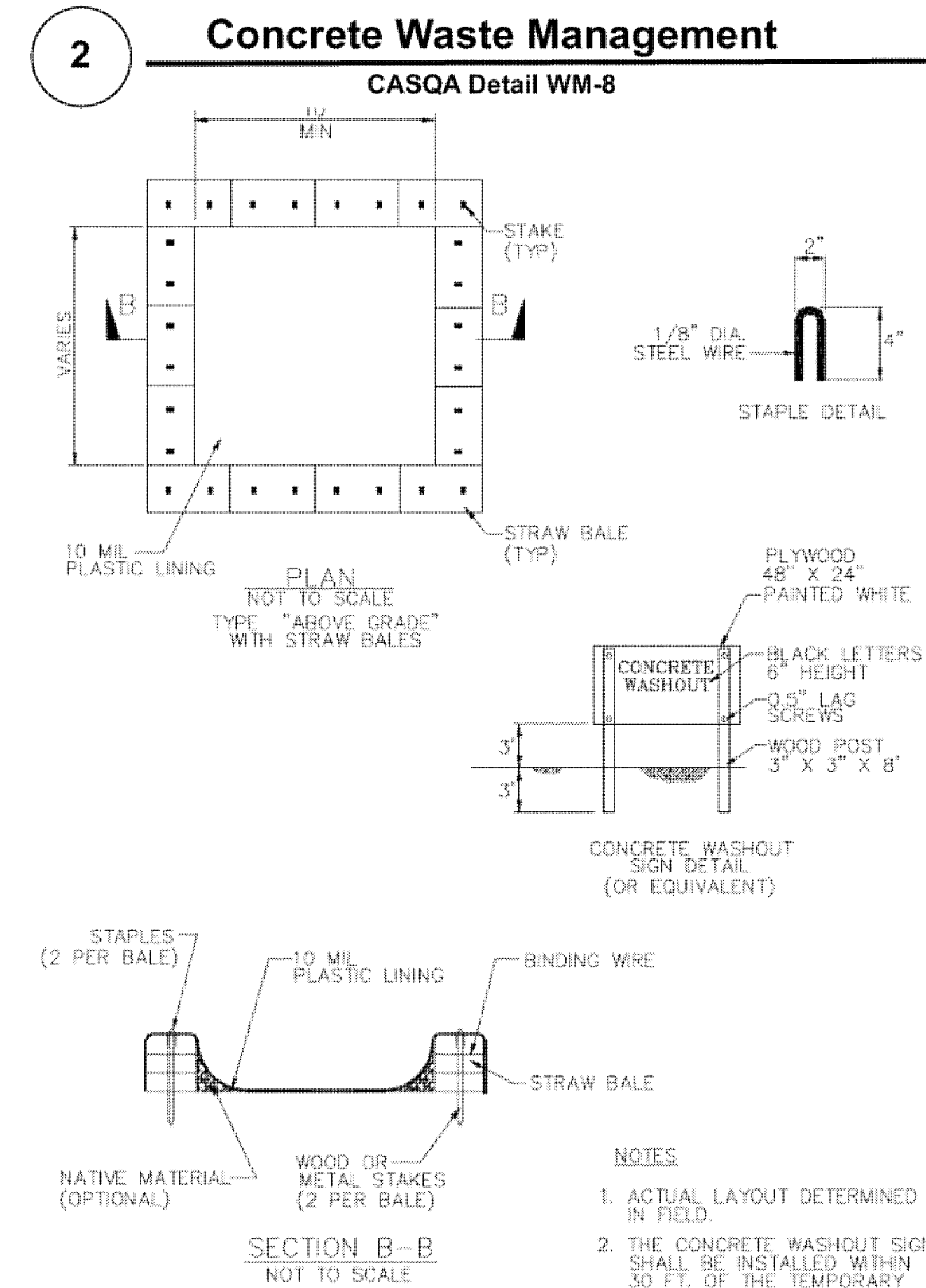
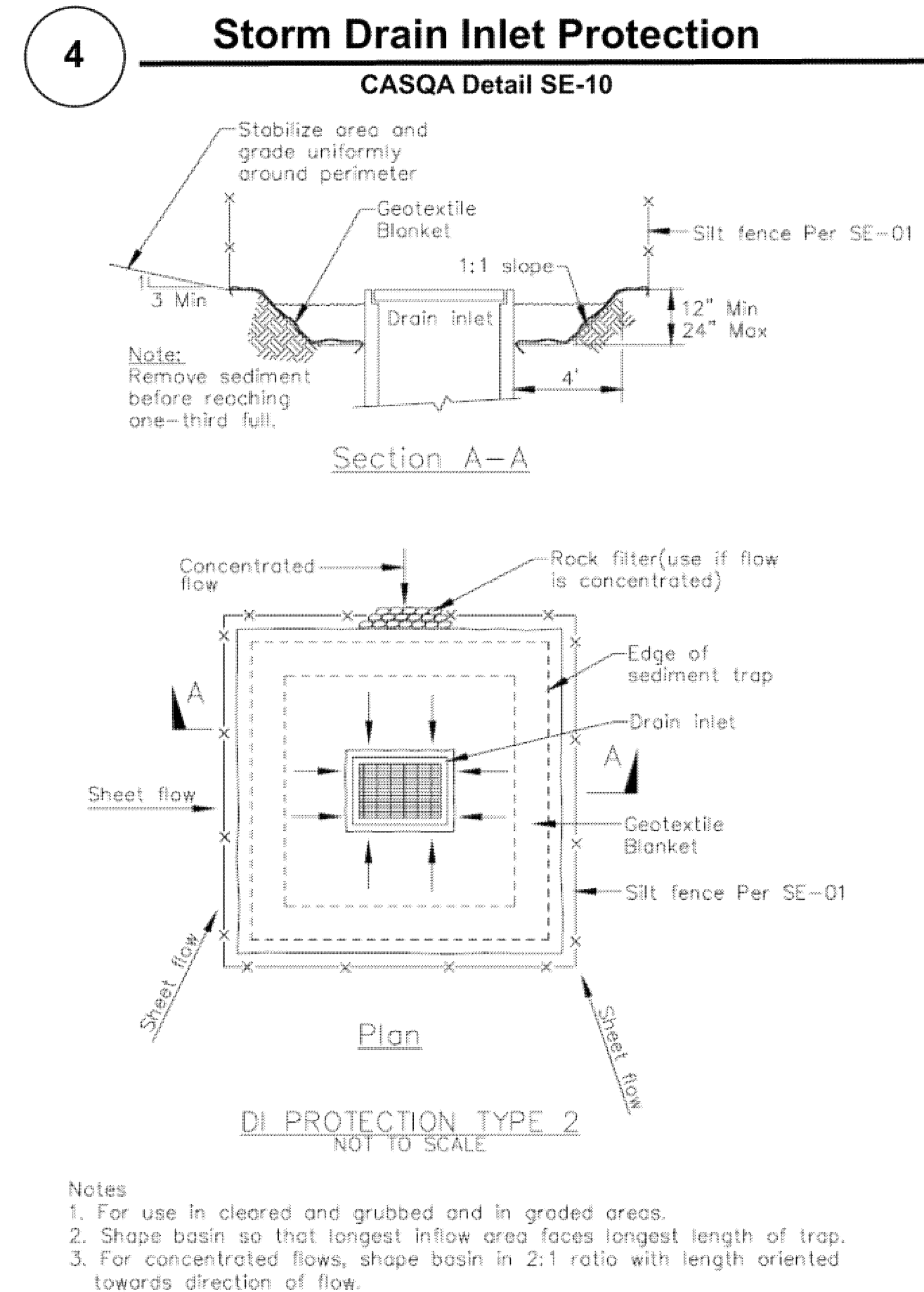
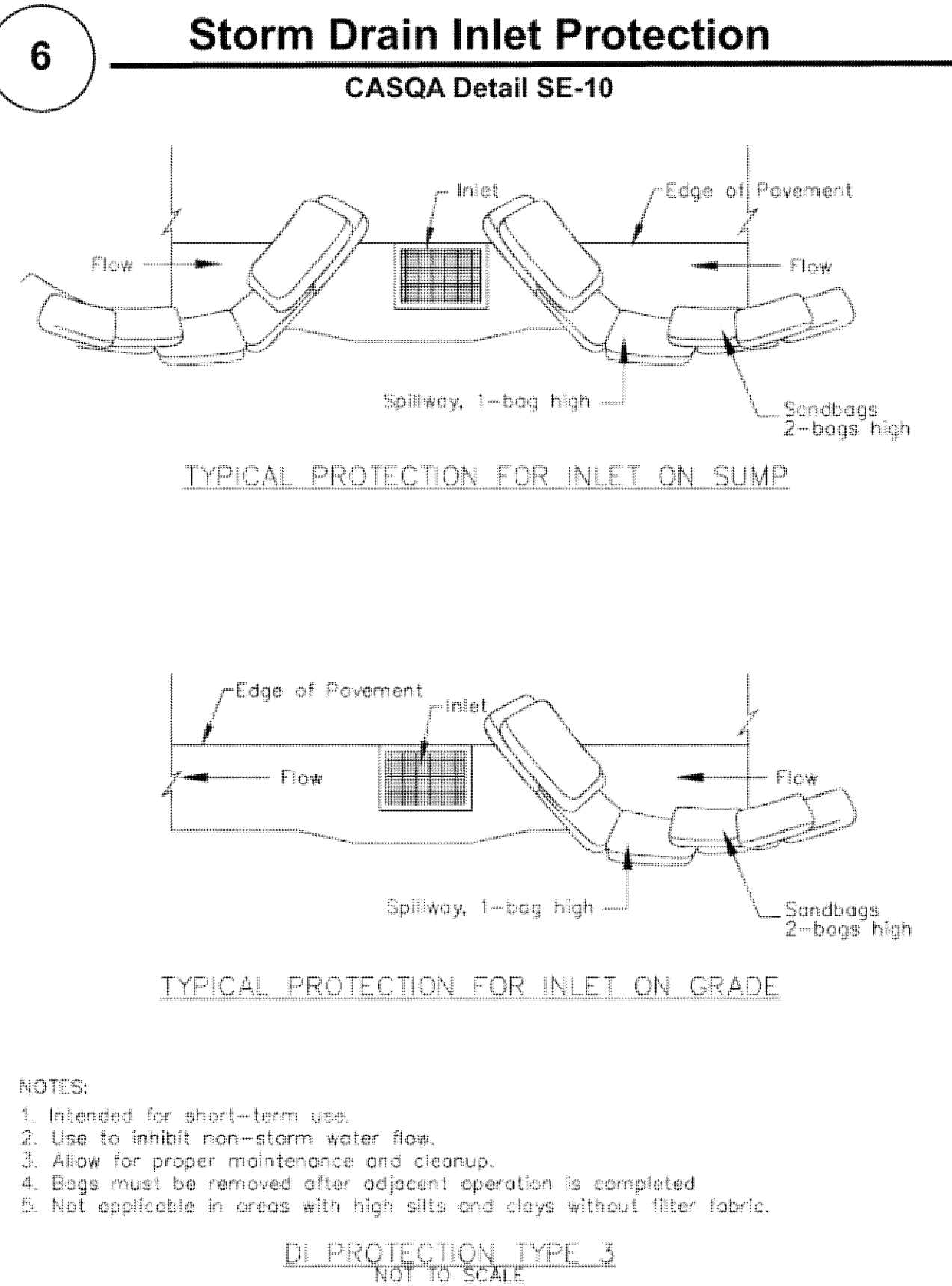
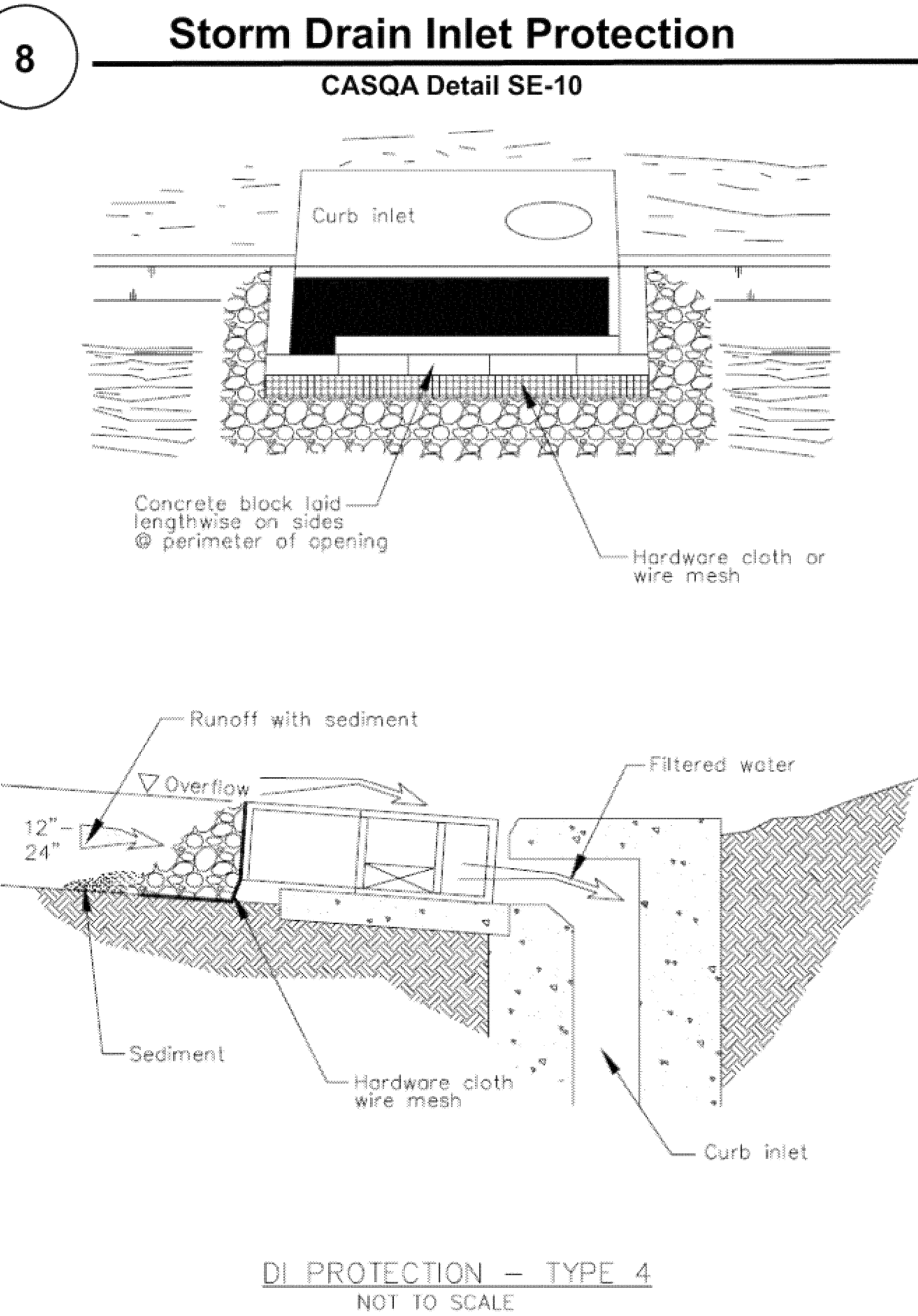
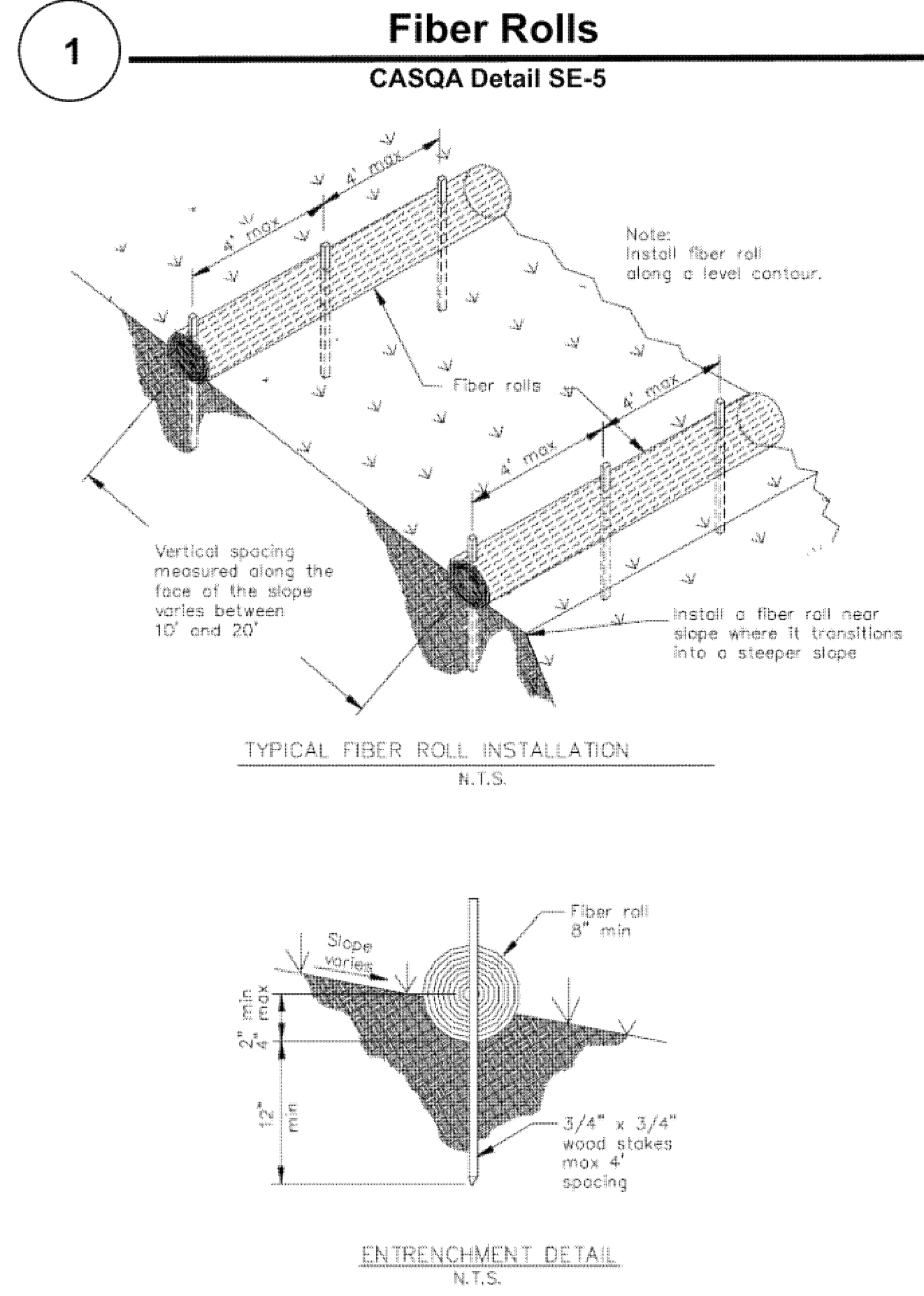
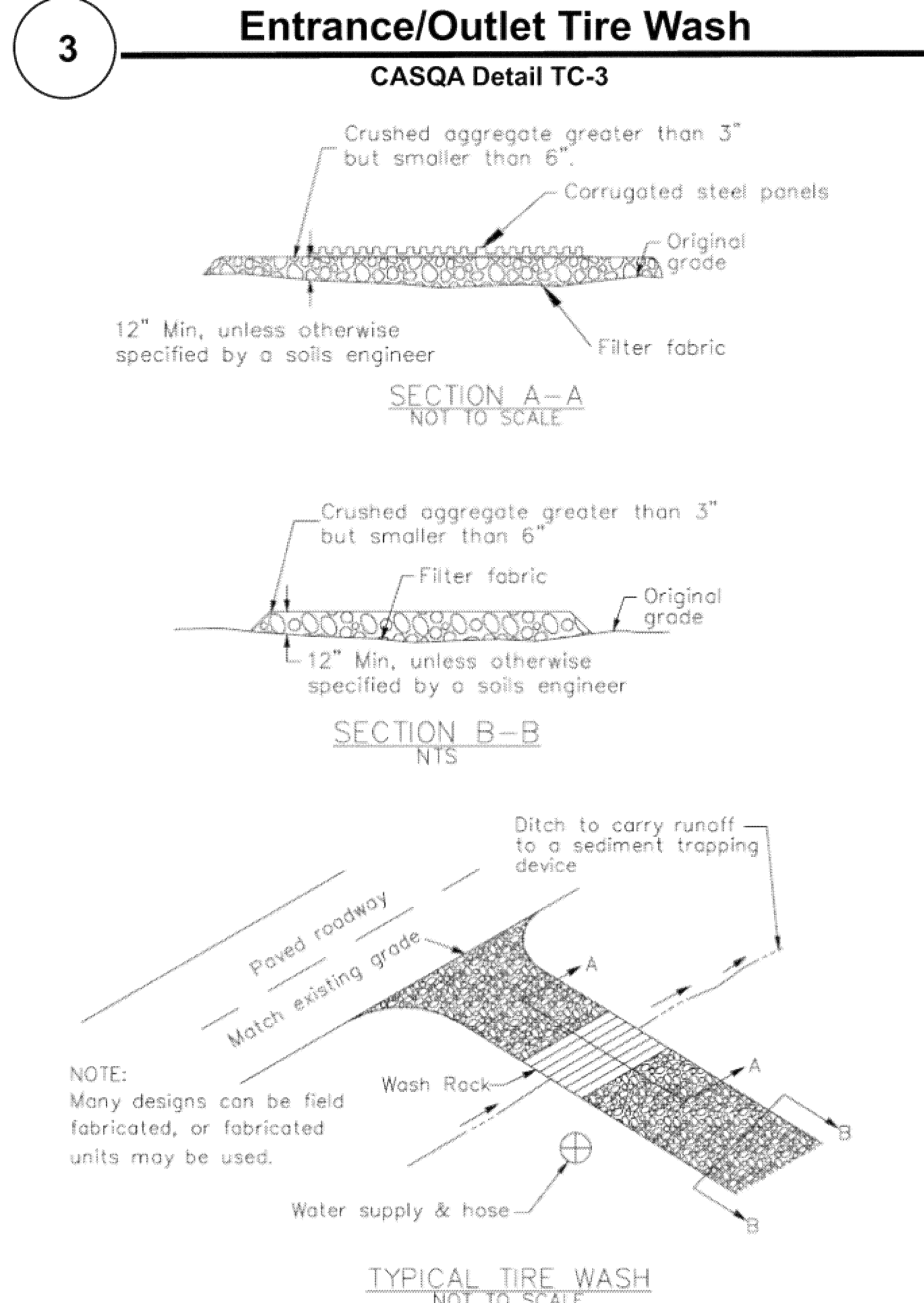
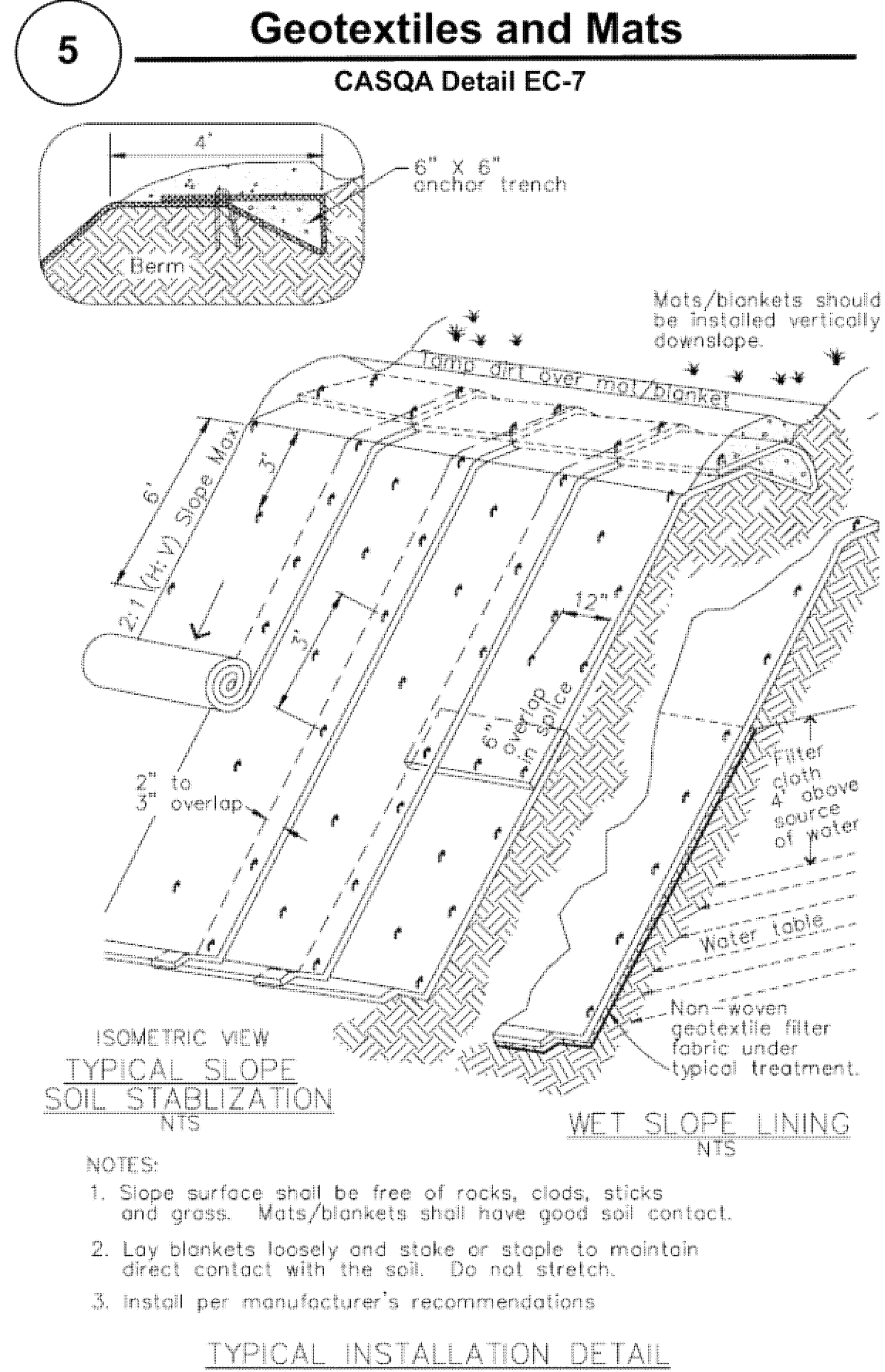
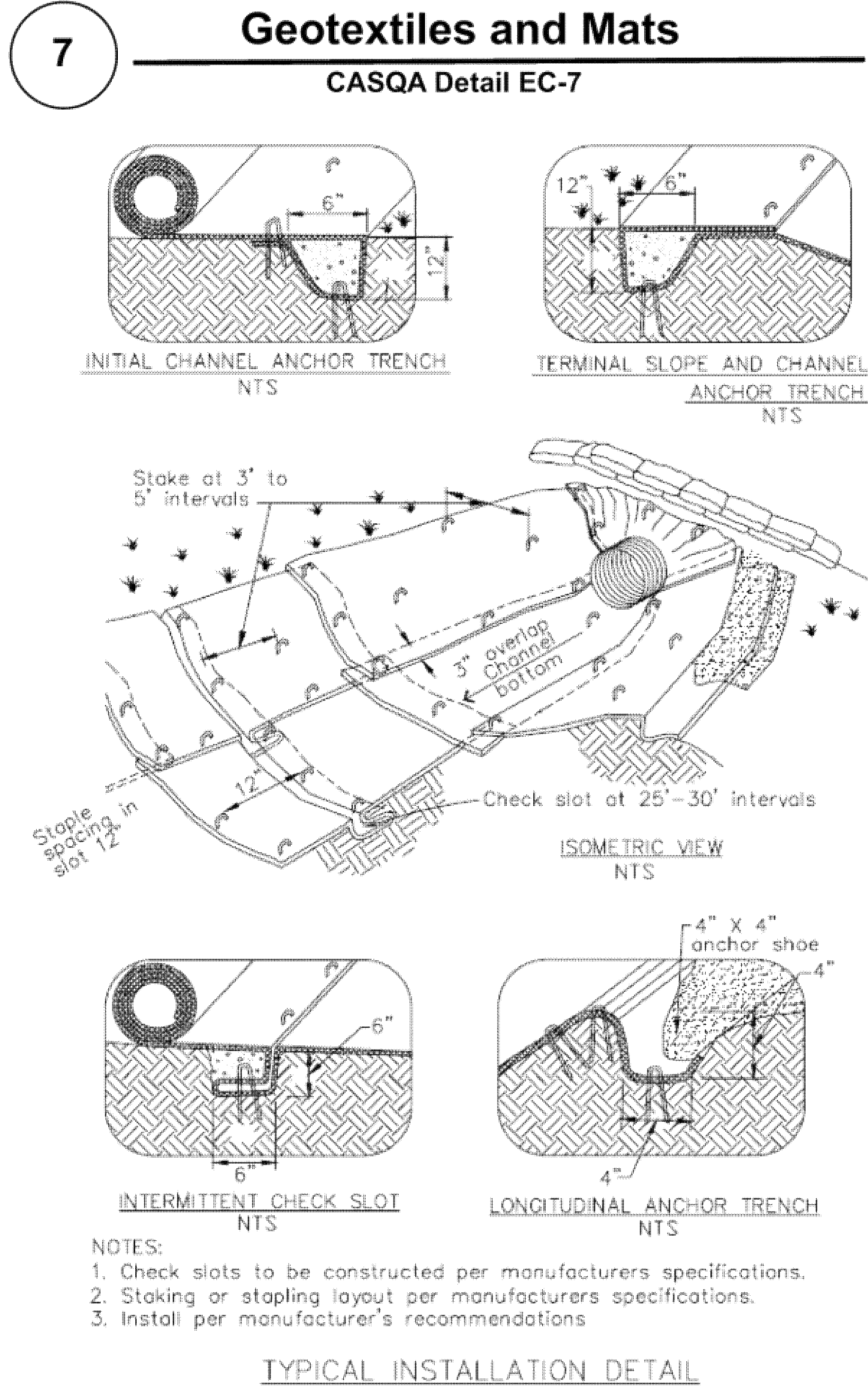
SANTA CLARA COUNTY, CALIFORNIA

A.P.N.: 728-38-001

Project Information







Source for Graphics: California Stormwater BMP Handbook, California Stormwater Quality Association, January 2003.  
Available from [www.cabmphandbooks.com](http://www.cabmphandbooks.com).

