

NEW RESIDENCE

15581 GLEN UNA DRIVE, LOS GATOS, CA 95030
APN# 510-26-007

PROJECT INFORMATION

OWNER: KALYANI & MURTHY VUPPALA
OWNER'S ADDRESS: 15581 GLEN UNA DRIVE, LOS GATOS, CA 95030
APN: 510-26-007
ZONING: RHS-D1
PROJECT SITE LOCATED WITHIN **SRA & WUI**- Very High
OCUPANCY GROUP: R-3 / U
BUILDING TYPE: TYPE V-B
NO OF STORIES (EXISTING): 2
NO OF STORIES (PROPOSED): 2
CURRENT USE: SINGLE FAMILY DWELLING
PROPOSED USE: SINGLE FAMILY DWELLING + ADU
LOT AREA: **33,120 Sq.Ft., +/- (GROSS)**
30,890 Sq.Ft., +/- (NET)
MAX. BUILDING HEIGHT: - (ALLOWED 35' MAX.)

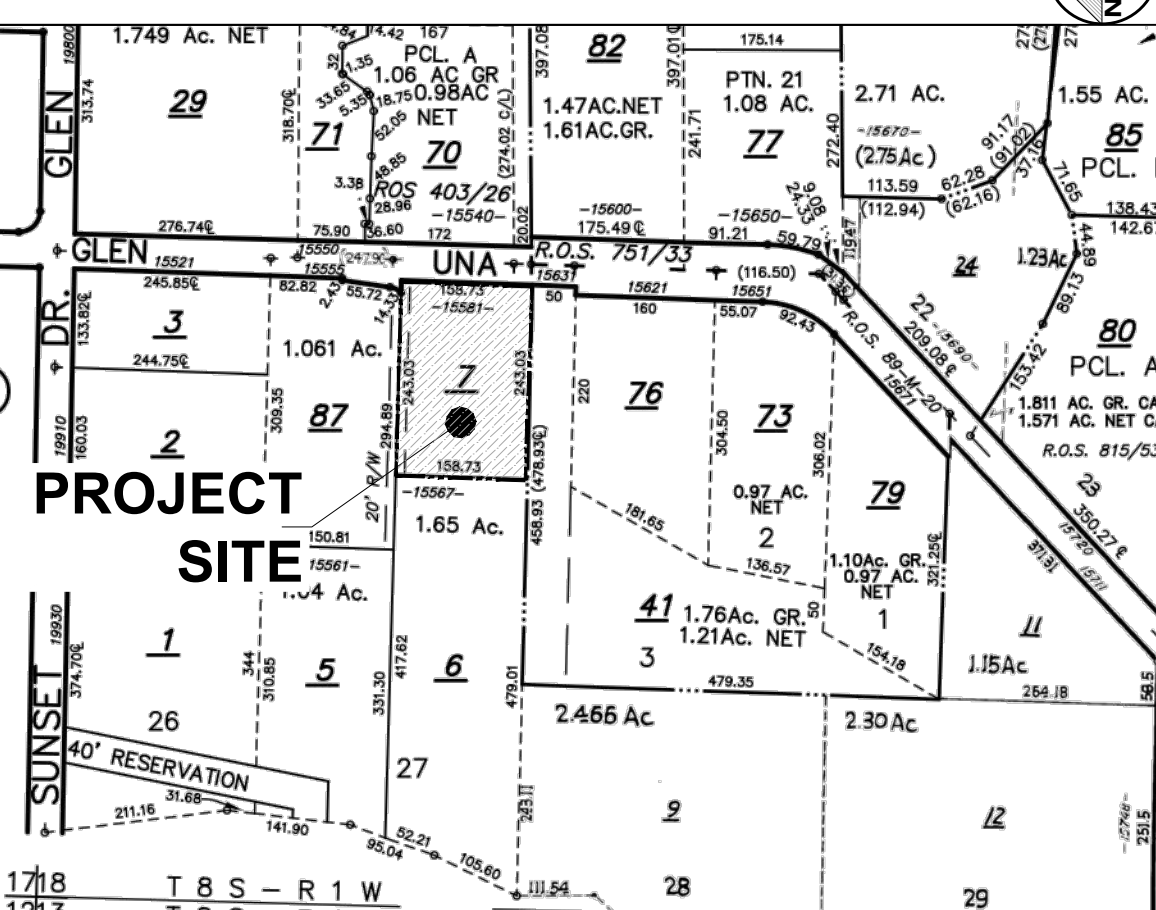
GOVERNING CODES/ JURISDICTION:

SANTA CLARA COUNTY, CA
2019 California Building Code
2019 California Residential Code
2019 California Plumbing Code
2019 California Mechanical Code
2019 California Electrical Code
2019 California Fire Code
2019 California Energy Code
2019 CalGreen Code

ALONG WITH ANY OTHER APPLICABLE LOCAL COUNTY AND STATE LAWS AND REGULATIONS

SETBACKS	PROPOSED
MINIMUM	MAIN HOUSE
FRONT 30 Ft.	47'-0"
SIDE 20 Ft.	38'-0" (Left); 30'-1" (Right)
REAR 25 Ft.	34'-0"

PARCEL MAP



VICINITY MAP



SCOPE OF WORK

PROJECT COMPRISES OF COMPLETE TEARDOWN OF EXISTING TWO STORY RESIDENCE AND AN ACCESSORY GARAGE STRUCTURE ON SITE AND CONSTRUCTION OF NEW TWO STORY DWELLING WITH ATTACHED 3 CAR GARAGE (~12,391 SQ.FT.) AND A NEW DETACHED ADU (~1132 SQ.FT.) THAT INCLUDES:
EXISTING:
* REMOVE EXISTING 2 STORY RESIDENCE INCLUDING ACCESSORY GARAGE STRUCTURE TOWARDS THE REAR OF THE SITE.
* REMOVE EXISTING SWIMMING POOL.
PROPOSED:
MAIN RESIDENCE- NEW TWO-STORY **SINGLE-FAMILY** RESIDENCE (8 BEDROOM, 10 BATH) WITH AN ATTACHED 3-CAR GARAGE
ADU- NEW DETACHED SINGLE STORY ADU (2 BEDROOM, 2 BATH) TOWARDS THE REAR OF THE PROPERTY.
PROJECT SITE LOCATED WITHIN STATE RESPONSE AREA (SRA) & WILDLAND URBAN INTERFACE (WUI)- Very High (REFER ADDITIONAL NOTES ON SHEET A0.1)
DEFERRED SUBMITTAL- FIRE SPRINKLER SYSTEM.

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BUILDING AREA SUMMARY

See Sheet A2.2 for Floor Area Calculations

FAR SUMMARY INFORMATION	
Gross Lot Area	33120 Sq.Ft.
Net Lot Area	30890 Sq.Ft.
Floor Area Calculations (See Sheet A2.3 for Area Calcs)	
Primary Single Family Residence (SFR)	
1st Floor	6740 Sq.Ft.
2nd Floor	5684 Sq.Ft.
Attached Garage	760 Sq.Ft.
Covered Porches	1134 Sq.Ft.
GRAND TOTAL (Gross Floor Area)	14318 Sq.Ft.
F.A.R. (Gross Floor Area/ Net Lot Area)	0.46
ADU (Accessory Dwelling Unit): Excluded from FAR Calc	
Gross Floor Area	1115 Sq.Ft.

PROPOSED LOT COVERAGE = 9702 Sq.Ft. (29.29%)
(NEW MAIN RESIDENCE + NEW ADU)

GRADING CALCULATIONS
REFER GRADING PLAN SHEET C1.0

IMPERVIOUS AREA CALCULATIONS
REFER SHEET HYD-1

PROJECT DIRECTORY

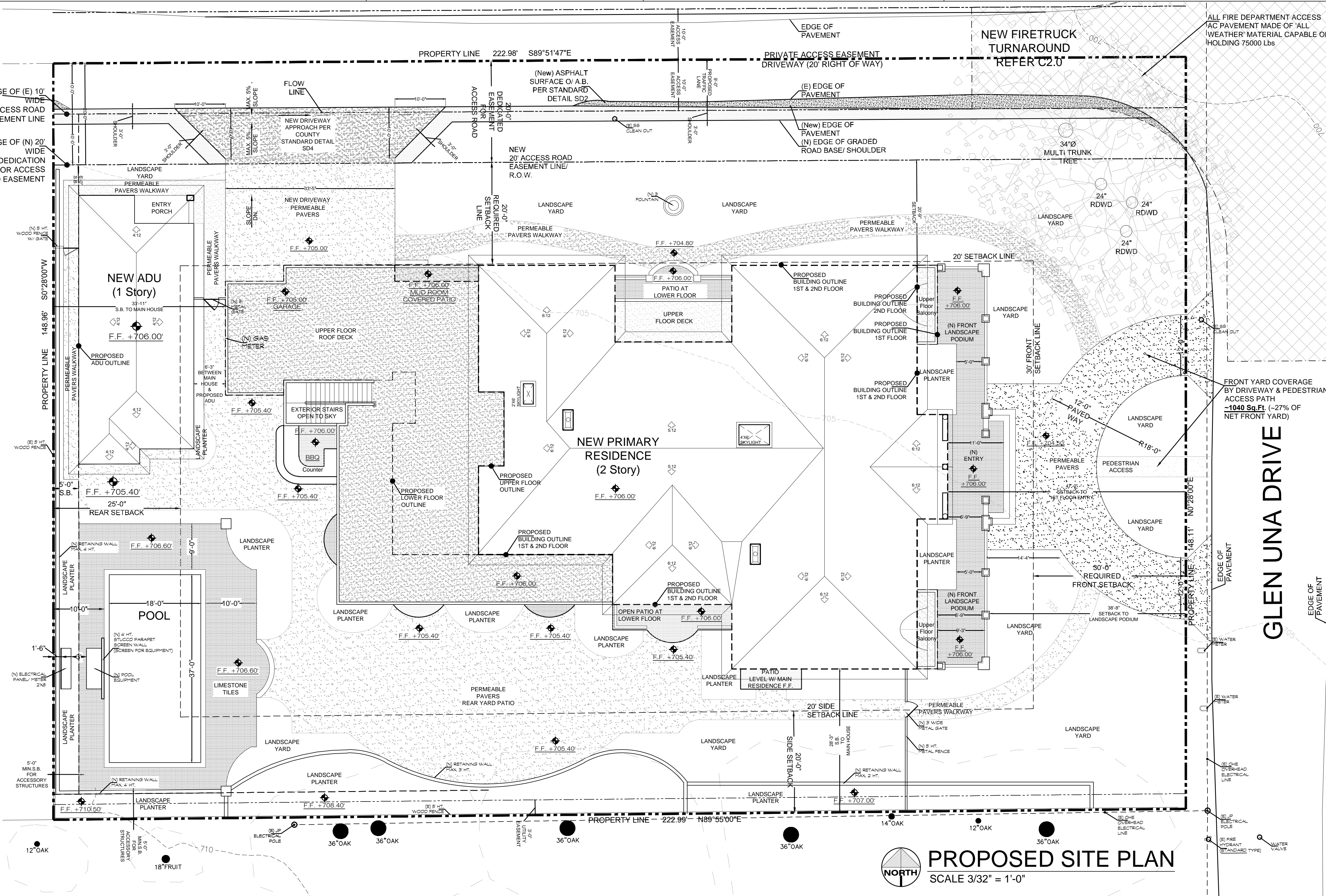
OWNER:
KALYANI & MURTHY VUPPALA
15581 Glen Una Drive,
Los Gatos, CA 95030
Tel: 408-544-0566
MURTHYVL@GMAIL.COM

DESIGNER:
ARTS DESIGNER INC
P.O. BOX 3426
Los Altos, CA 94024
Tel: 650-948-8492
ARTDESIGNER@COMCAST.NET

CIVIL ENGINEER:
Lea & Braze Engineering, Inc
Civil Engineers | Land Surveyors
HAYWARD - ROSEVILLE - DUBLIN - SAN JOSE
WWW.LEABRAZE.COM
Phone: 510-887-4086 x.117

LANDSCAPE ARCHITECT:
KAREN AITKEN
Karen Aitken & Associates
8262 Rancho Real, Gilroy, CA 95020
Tel: 408-842-0245
KAREN@KAA.DESIGN

ARCHITECT:
YOGESH CHITLANGIA
AADIARCH INC.
332 Torino Drive, Apt 3,
San Carlos, CA 94070
Tel: 650-560-5550
YOGESH@AADIARCH.COM

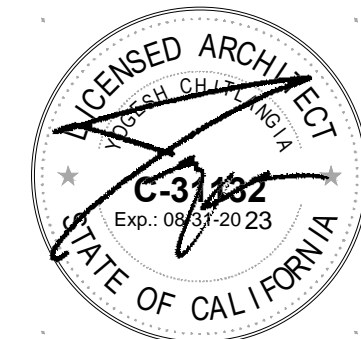


PROPOSED SITE PLAN
SCALE 3/32" = 1'-0"

NEW RESIDENCE

15581 GLEN UNA DRIVE,
LOS GATOS, CA 95030
APN# 510-026-007

Date	Issued For
10.27.22	PLANNING
12.03.22	PLANNING (Rev 1)



TITLE:
COVER SHEET, INDEX OF SHEETS, PROJECT INFO., SITE PLAN

PROJECT NO:
2021-04-13

DRAWING NO:

A 0.0

15581 GLEN UNA DRIVE,
LOS GATOS, CA 95030
APN# 510-026-007

<u>Date</u>	<u>Issued For</u>
07.01.22	PLANNING
11.14.22	PLANNING (Rev 1)

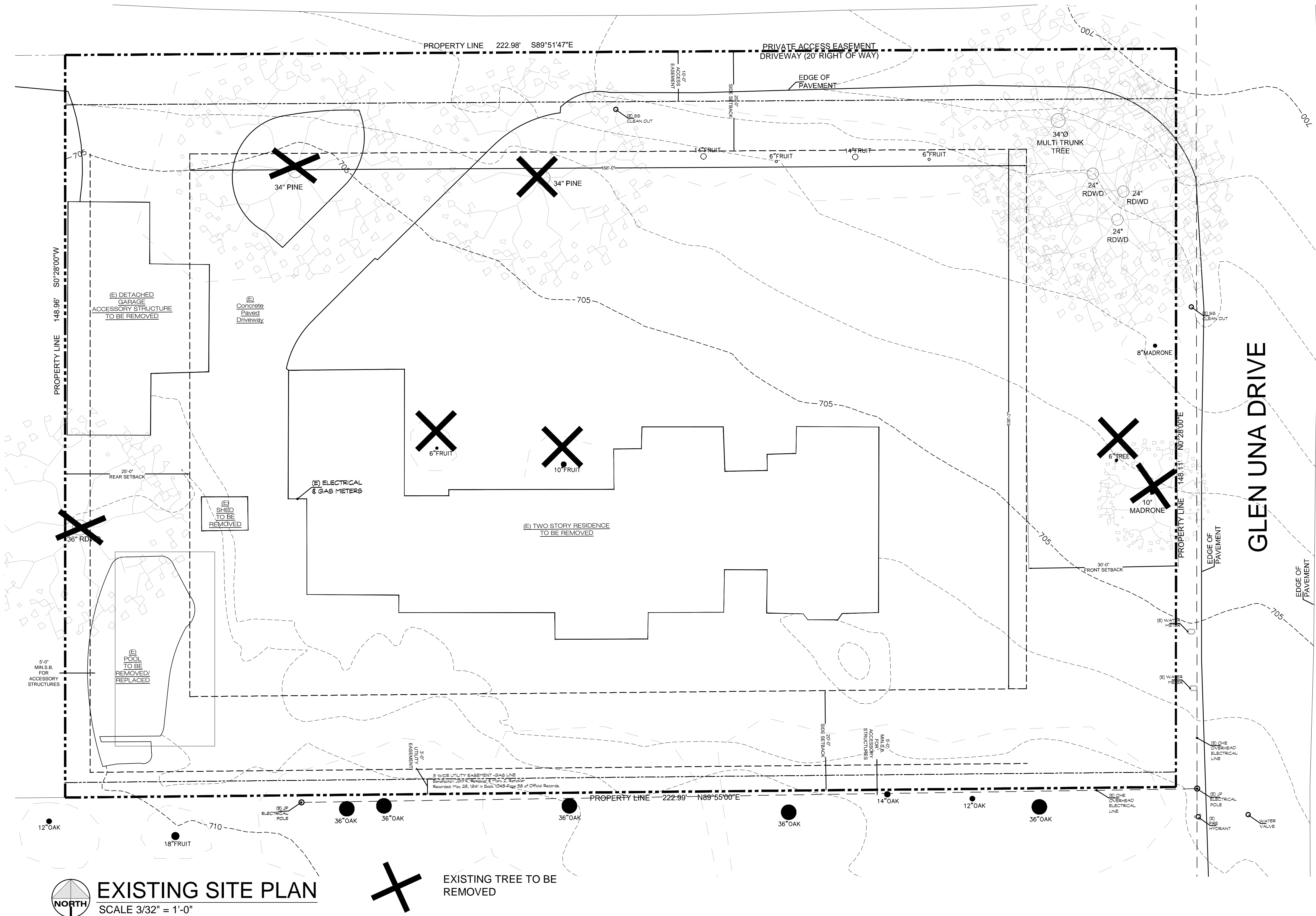


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EXISTING SITE PLAN

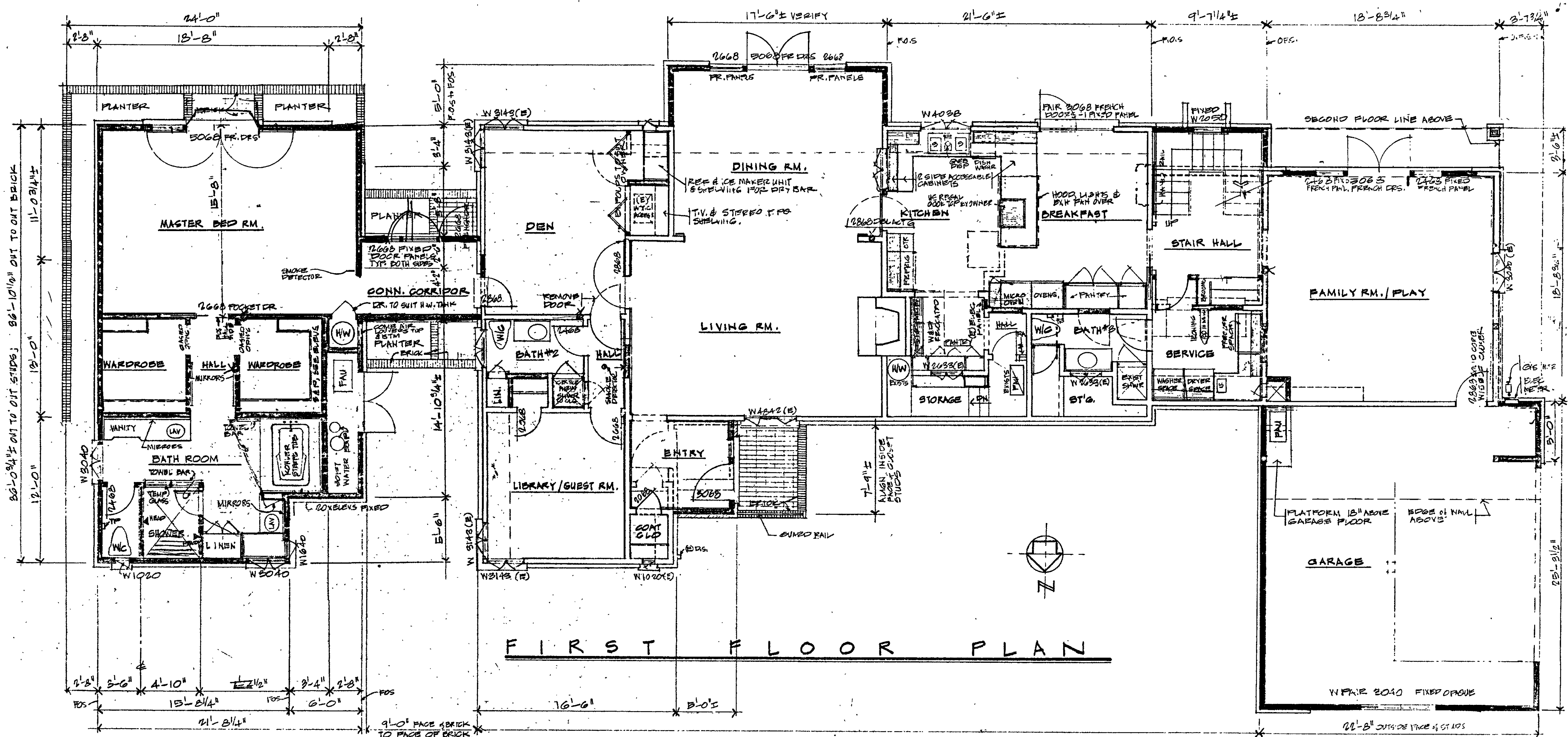
PROJECT NO:
2021-04-13

DRAWING NO:

A 1.0



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NOTE:
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YOGESH CHITLANGIA
Architect/ Designer

332 Torino Drive, #3, SAN CARLOS,
CA 94070

TEL: (650)-560-5550
FAX: (650)-560-5551
YOGESH@AADIARCH.COM

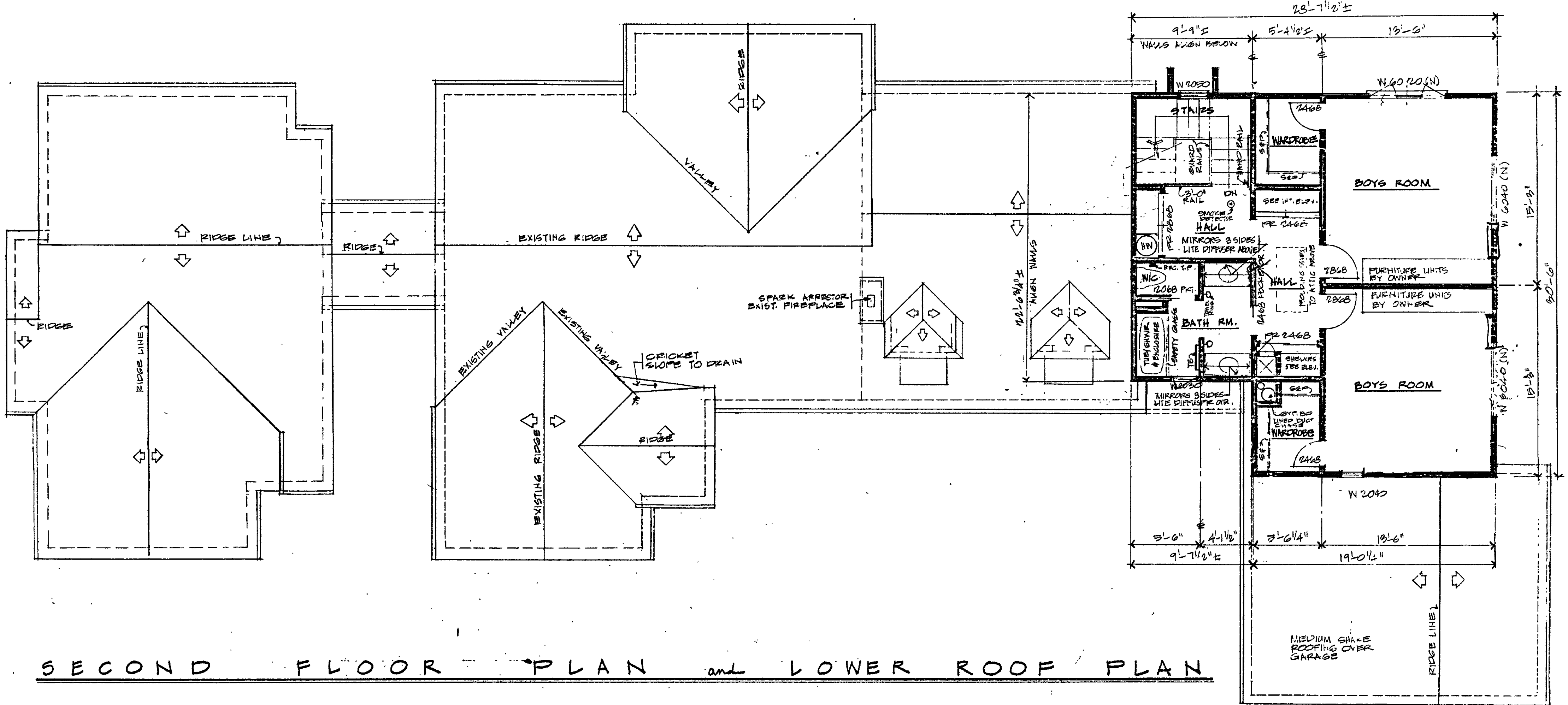
Arts Designer, Inc
NADER GAWARGY
Los Altos, CA 94024
Tel: 650-948-8492



ALL EXISTING STRUCTURE TO BE REMOVED

1st FLR PLAN (EXISTING)

SCALE 3/16" = 1'-0"



ALL EXISTING STRUCTURE TO BE REMOVED

2nd FLR PLAN (EXISTING)

SCALE 3/16" = 1'-0"

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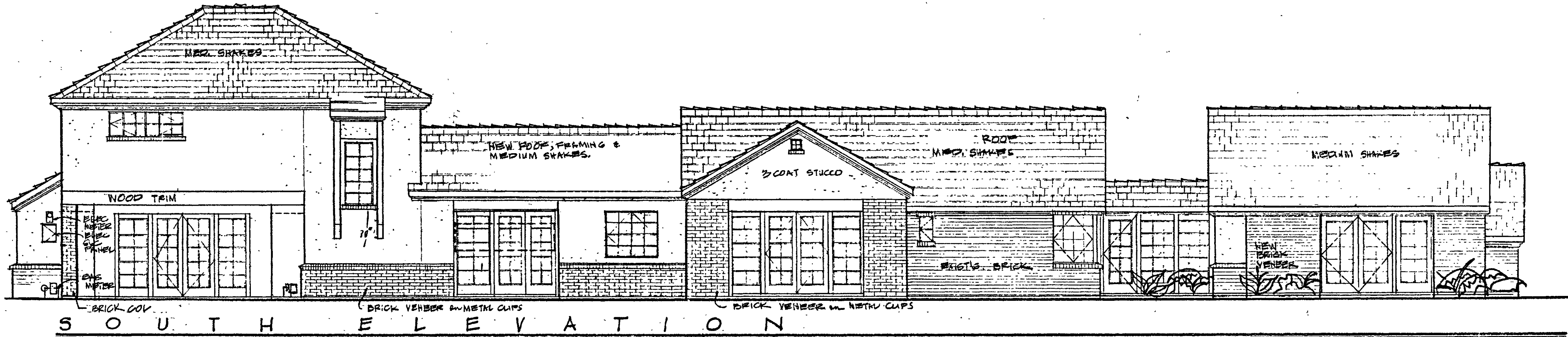
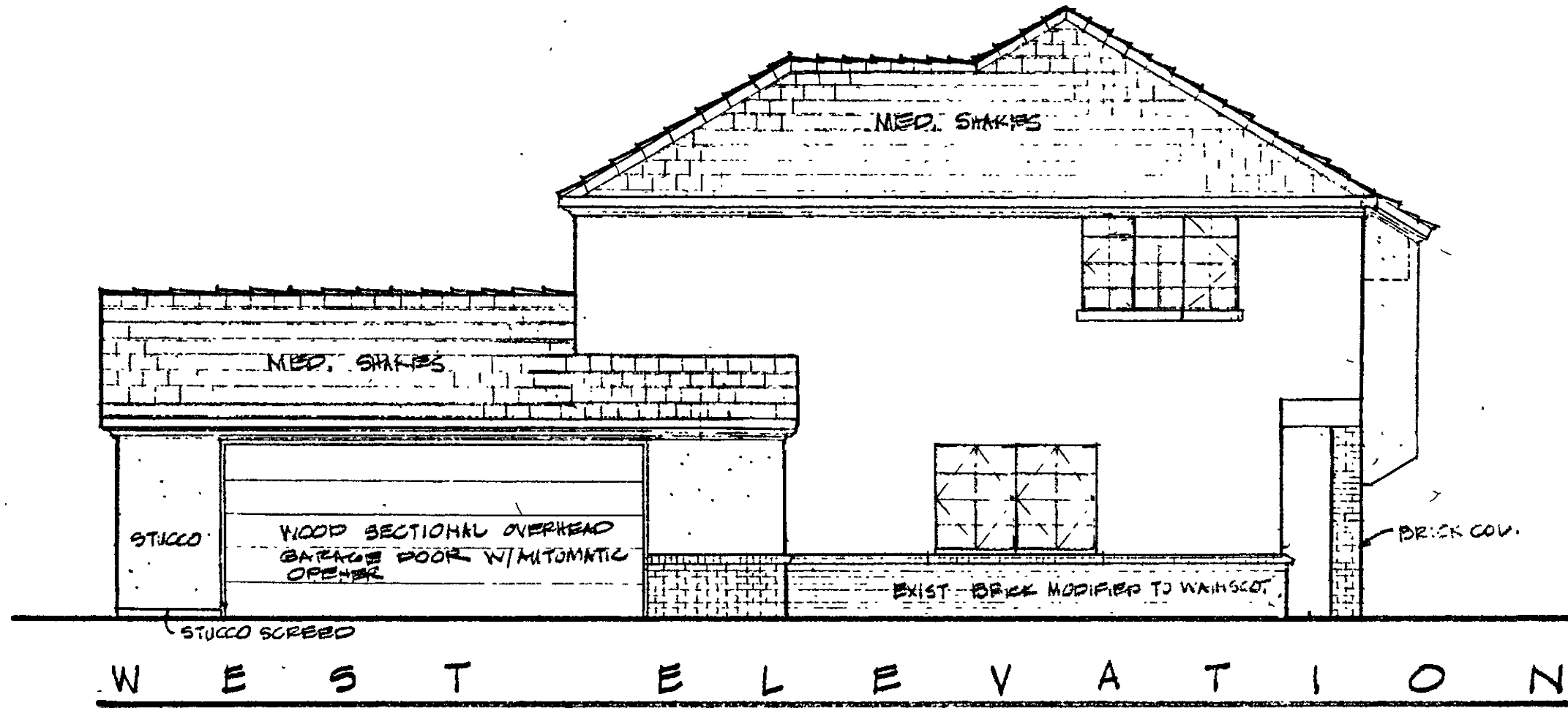
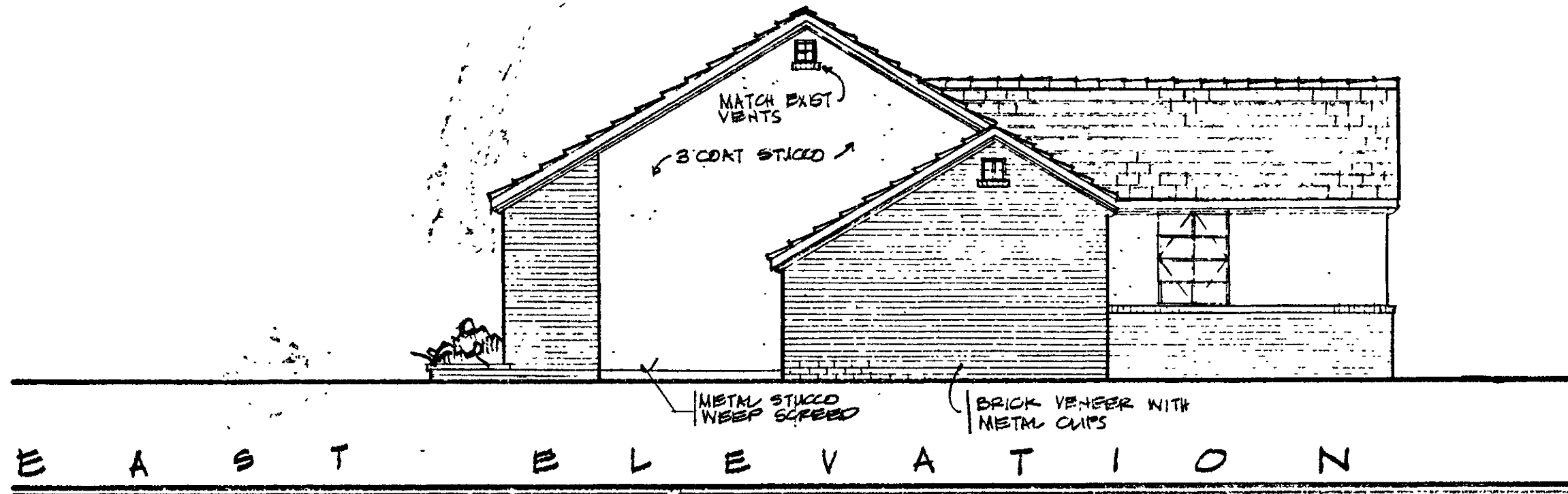
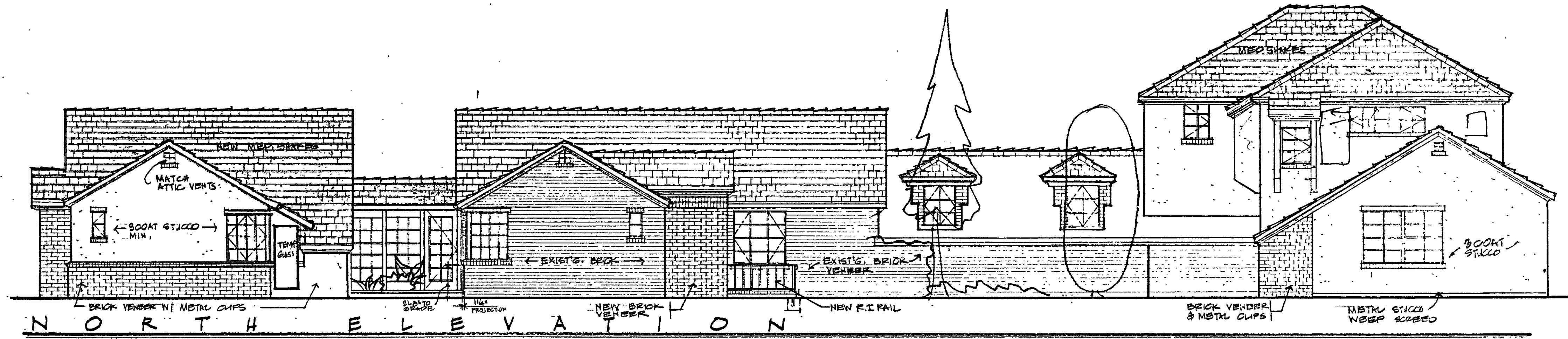
TITLE:
EXISTING FLOOR PLAN;
DEMOLITION PLAN

PROJECT NO:
2021-04-13

DRAWING NO:

A 1.1

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YOGESH CHITLANGIA
Architect/ Designer

332 Torino Drive, #3, SAN CARLOS,
CA 94070

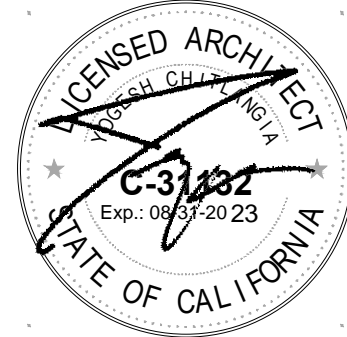
TEL: (650)-560-5550
FAX: (650)-560-5551
YOGESH@AADIARCH.COM

Arts Designer, Inc
NADER GAWARGY
Los Altos, CA 94024
Tel: 650-948-8492

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07.01.22	PLANNING



TITLE:
EXISTING ELEVATIONS
(REFERENCE)

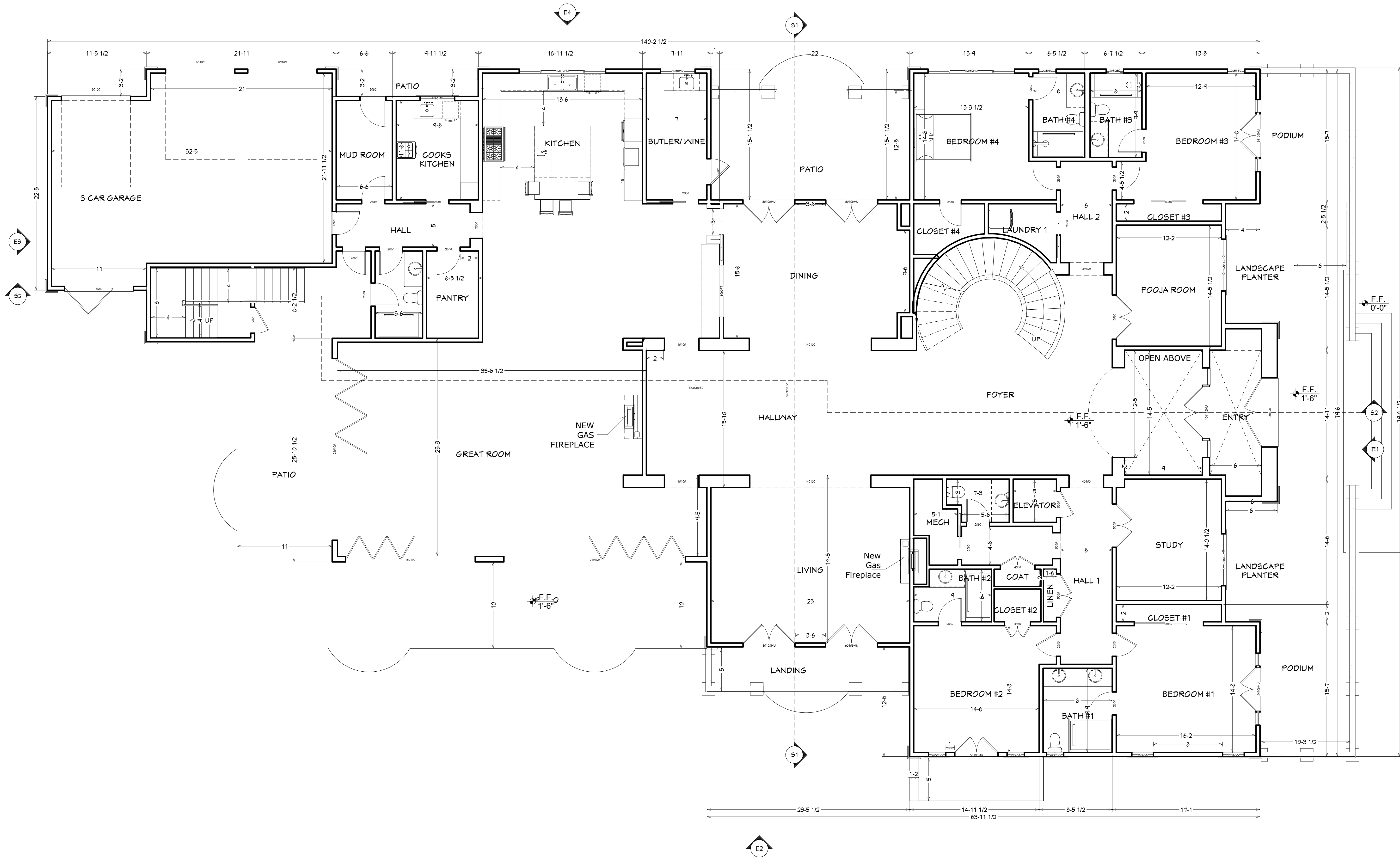
PROJECT NO:
2021-04-13

DRAWING NO:

A 1.2

EXISTING ELEVATIONS (REFERENCE)
SCALE 3/16" = 1'-0"

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1st FLOOR PLAN
Scale 3/16"= 1'-0"

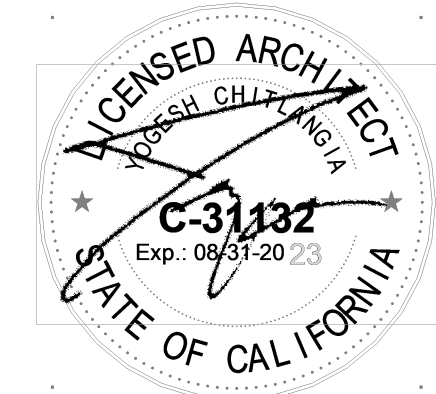


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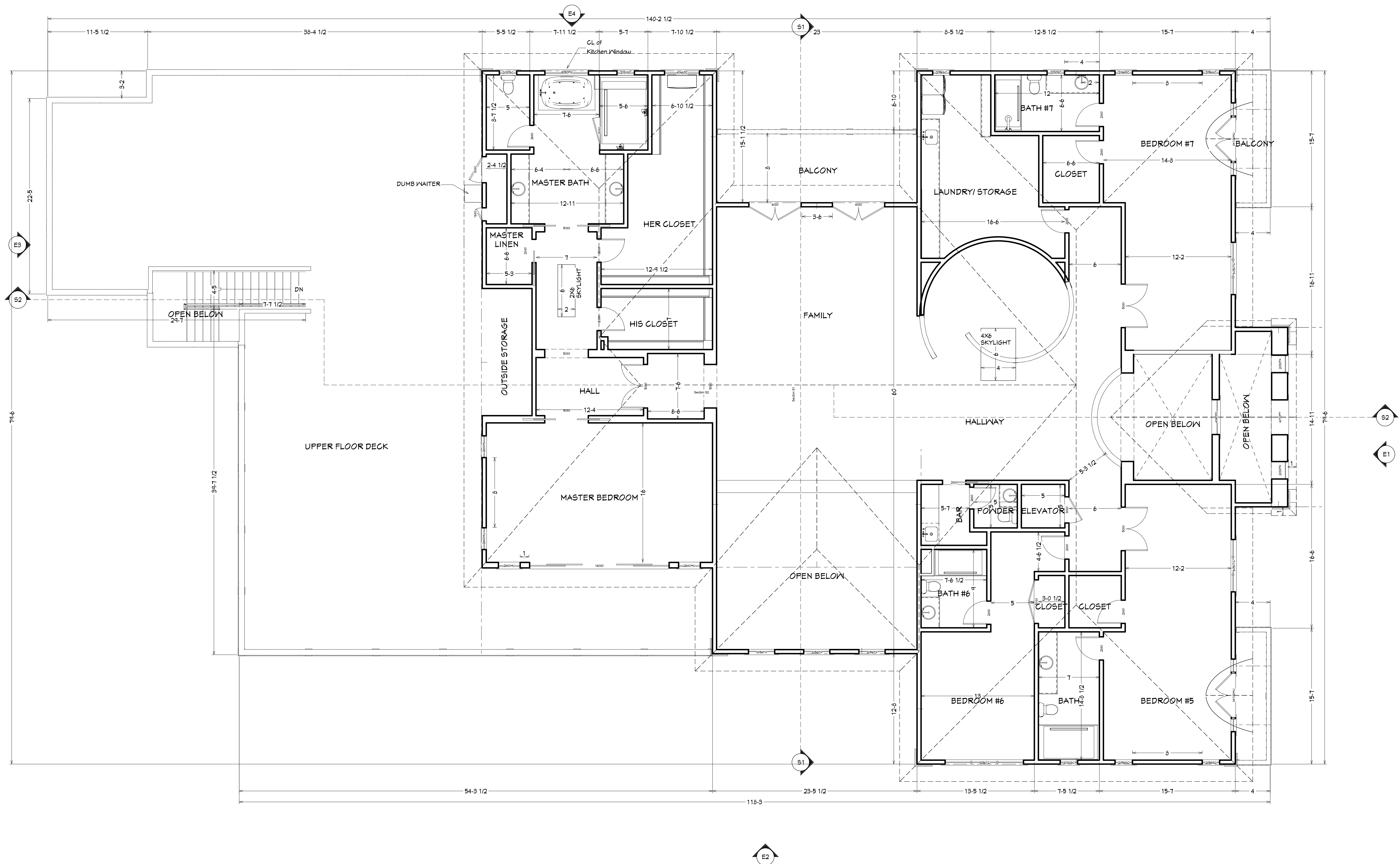
TITLE:
MAIN RESIDENCE
PROPOSED LOWER
FLOOR PLAN

PROJECT NO:
2021-04-13

DRAWING NO:

A 2.0

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UPPER FLOOR PLAN
Scale 3/16"=1'-0"

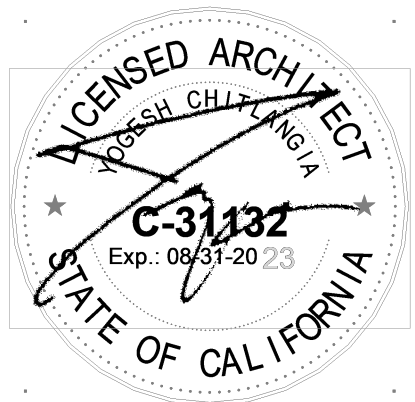


YOGESH CHITLANGIA
Architect/ Designer
332 Torino Drive, #3, SAN CARLOS,
CA 94070
TEL: (650)-560-5550
FAX: (650)-560-5551
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Art Designer, Inc.
NADER GAWARY
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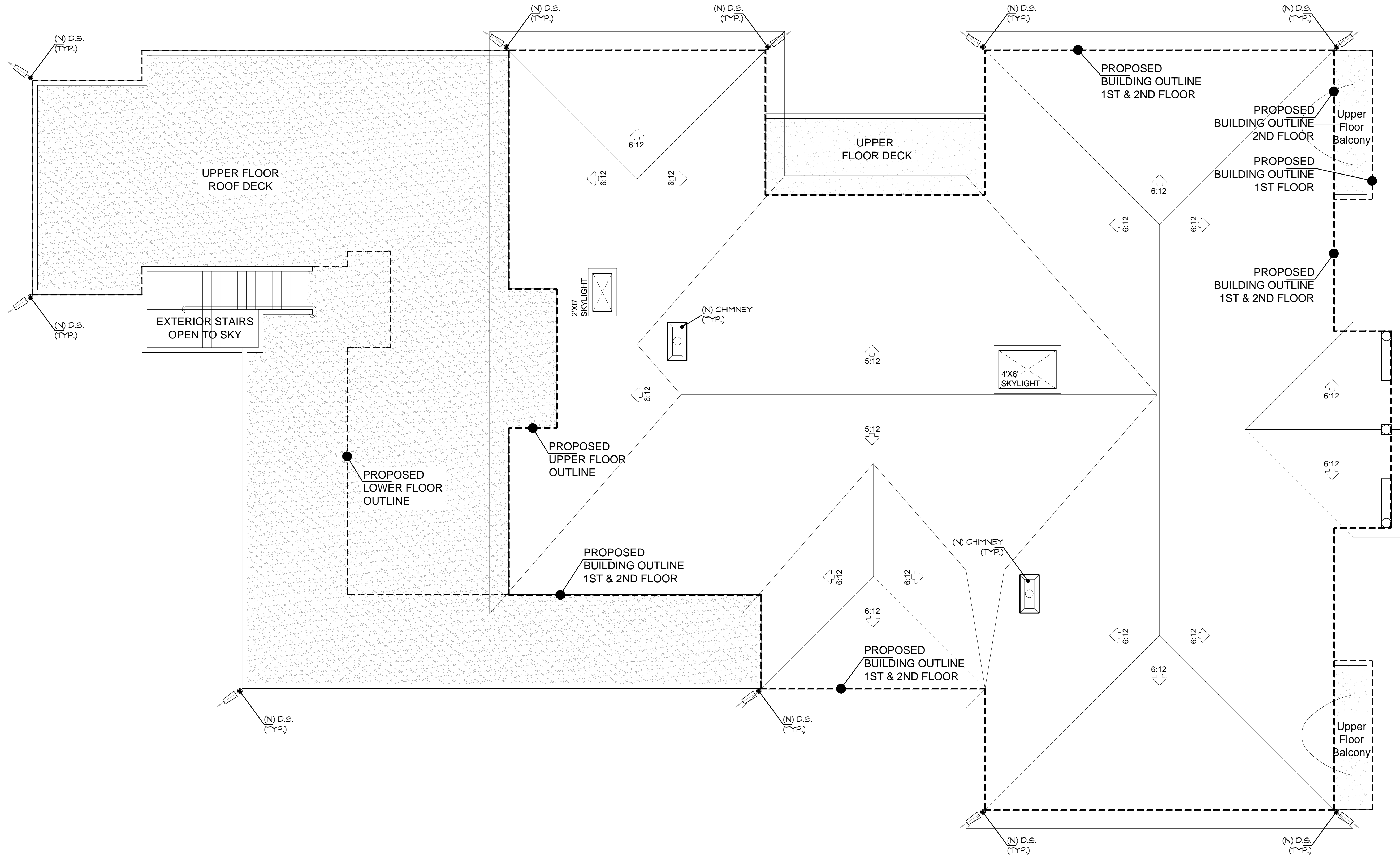
TITLE:
MAIN RESIDENCE
PROPOSED UPPER
FLOOR PLAN

PROJECT NO:
2021-04-13

DRAWING NO:

A 2.1

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PROPOSED ROOF PLAN- MAIN RESIDENCE
SCALE 3/16" = 1'-0"



YOGESH CHITLANGIA
Architect/ Designer

332 Torino Drive, #3, SAN CARLOS,
CA 94070

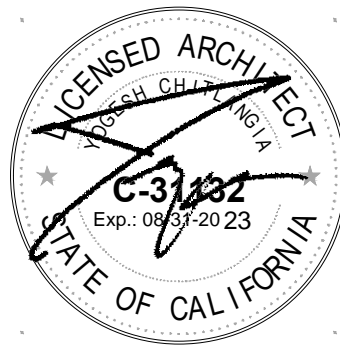
TEL: (650)-560-5550
FAX: (650)-560-5551
YOGESH@AADIARCH.COM

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APN# 510-026-007**

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TITLE:
ROOF PLAN- MAIN RESIDENCE

PROJECT NO:
2021-04-13

DRAWING NO:

A 2.2

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CAL FIRE NOTES:
PROJECT is located within the State Responsibility Area (SRA) and is recommended to follow all minimum wildfire protection standards of **California Code of Regulations Title 14 Natural Resources Division 1.5 Department of Forestry Chapter 7-Fire Protection Subchapter 2 SRA Fire Safe Regulations Articles 1-5.**

Sections of Glen Una Drive do not meet specifications in **§ 1273.01** in the Fire Safe Regulations. Those sections of Glen Una Drive will need to conform to **§ 1273.01** to Building site and provide a minimum of two ten (10) foot traffic lanes, not including shoulder and striping.

Defensible Space: Maintain defensible space specifications described in Public Resource Code 4291.

§1276.01. Setback for Structure Defensible Space.

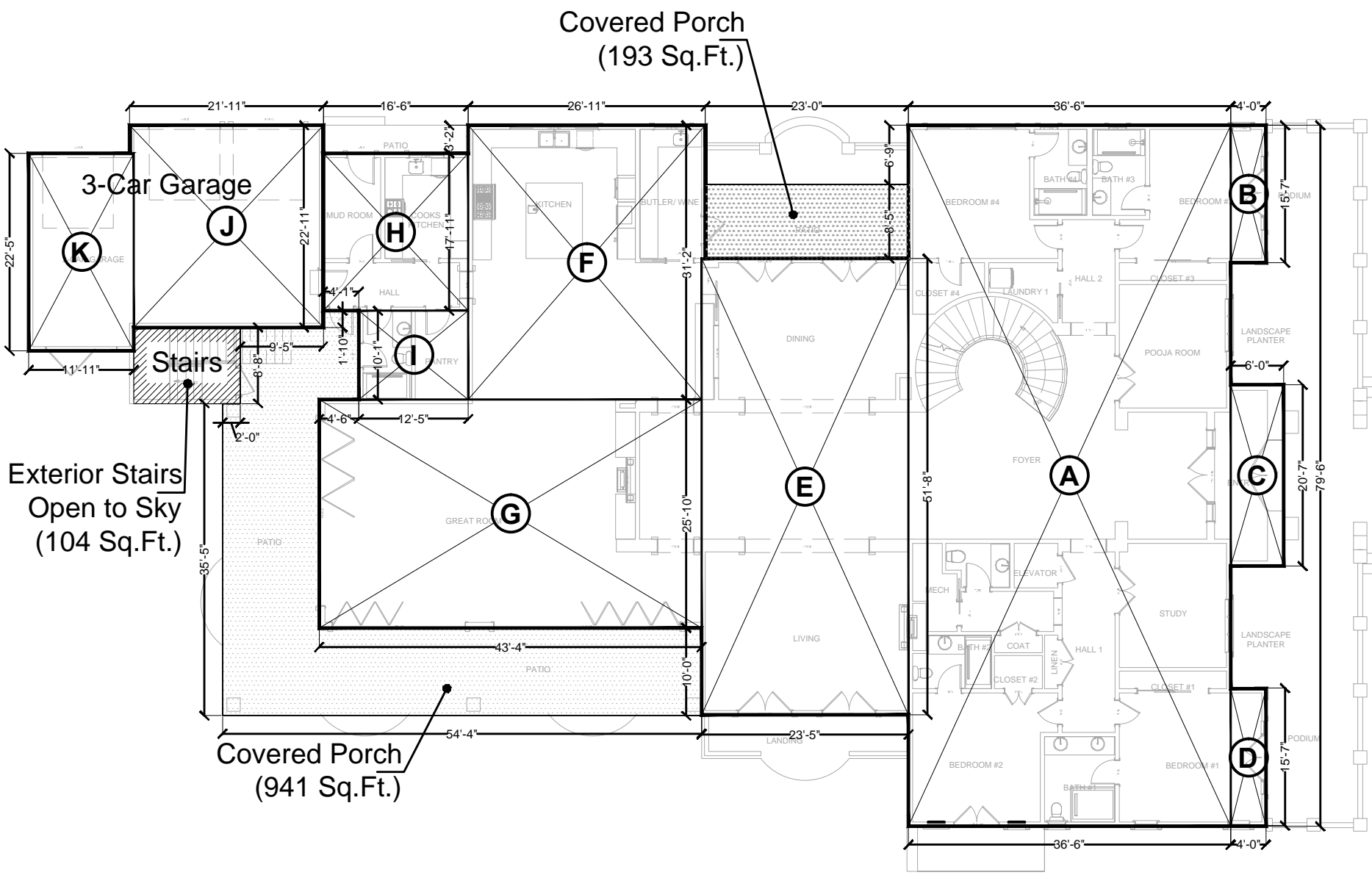
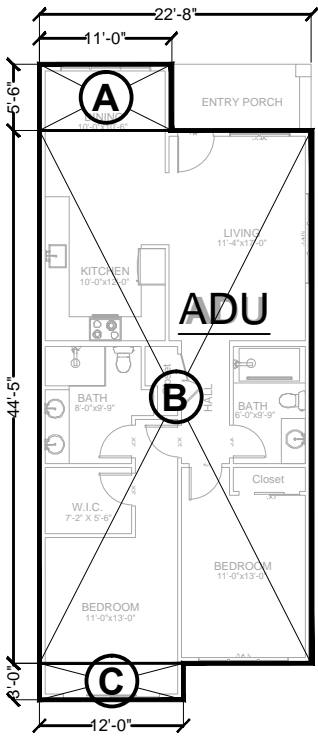
c. Structures constructed in the SRA are required to comply with the defensible space regulations in Title 14.

Natural Resources Division 1.5. Department of Forestry and Fire Protection Chapter 7. Fire Protection Subchapter

3. Fire Hazard.

FLOOR AREA CALCULATIONS
ADU (Accessory Dwelling Unit)
Scale 1/16"= 1'-0"

LIVING AREA (Conditioned)		
Area	Size	Area (Sq.Ft.)
A	11'-11"x 3'-2"	38.00
B	22'-7"x 26'-7"	600.00
C	20'-1"x 7'-0"	140.00
D	22'-7"x 14'-11"	337.00
Total		1115.00
GRAND TOTAL		1115.00 Sq.Ft.



FLOOR AREA CALCULATIONS
1ST FLOOR- MAIN RESIDENCE
Scale 1/16"= 1'-0"

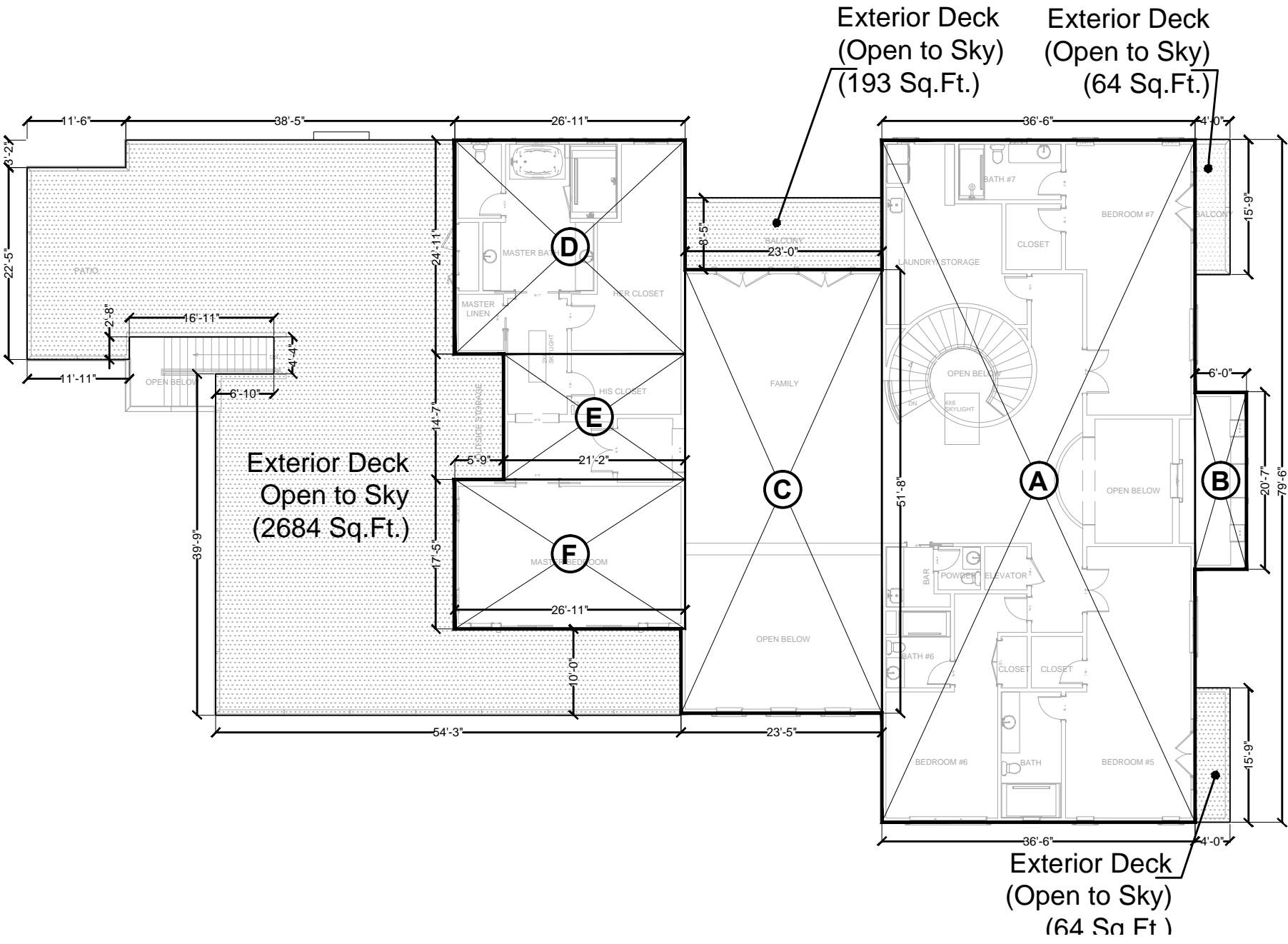
LIVING AREA (Conditioned)		
Area	Size	Area (Sq.Ft.)
A	36'-6"x 79'-6"	2902.00
B	4'-0"x 15'-7"	62.00
C	6'-0"x 20'-7"	123.00
D	4'-0"x 15'-7"	62.00
E	23'-5"x 51'-8"	1213.00
F	26'-11"x 31'-2"	839.00
G	43'-4"x 25'-10"	1119.00
H	16'-6"x 17'-11"	295.00
I	12'-5"x 10'-1"	125.00
Total		6740.00

GARAGE AREA		
Area	Size	Area (Sq.Ft.)
J	21'-11"x 22'-11"	502.00
K	11'-11"x 22'-5"	267.00
Total Garage		769.00

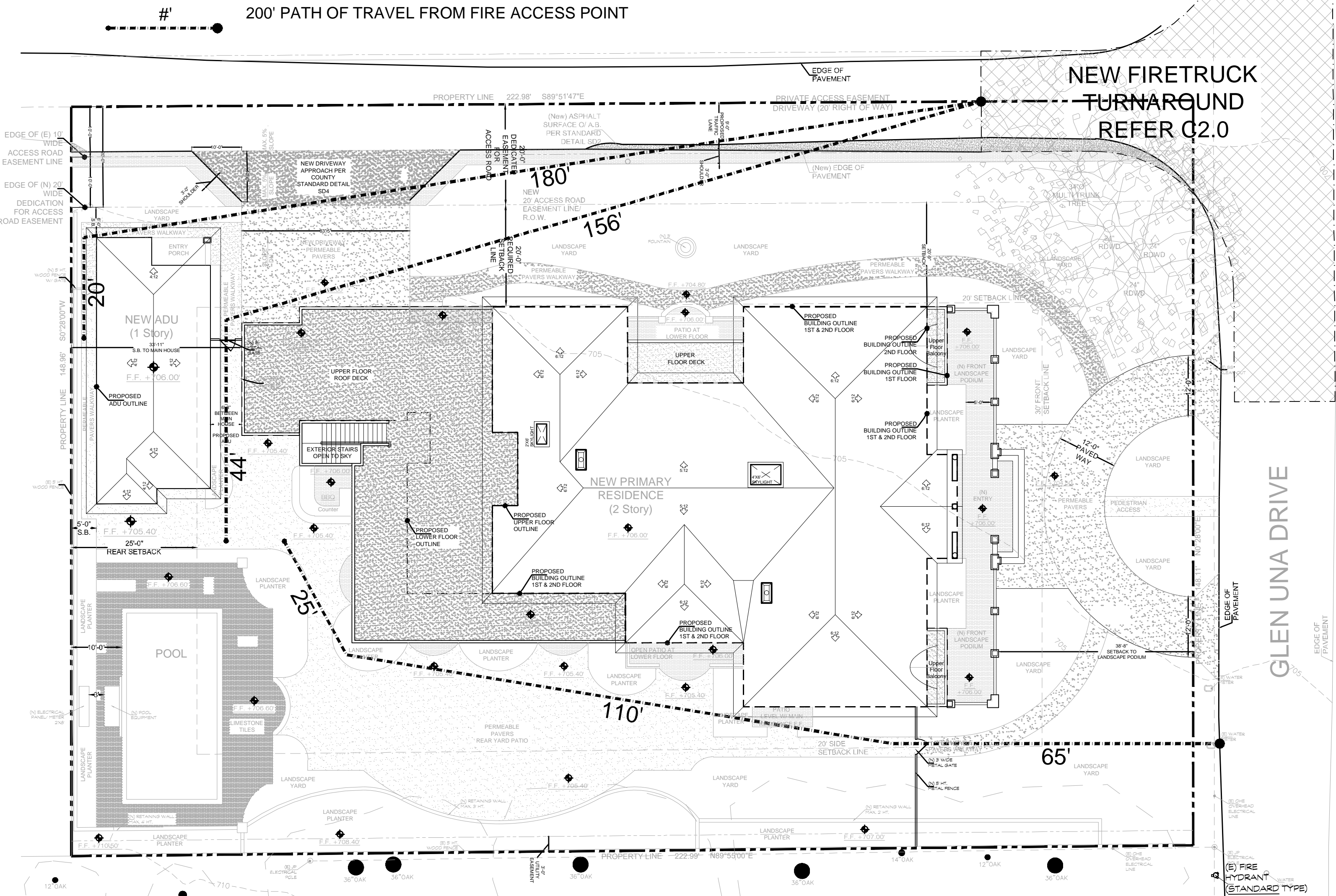
OTHER AREAS		
Area	Size	Area (Sq.Ft.)
Covered Porches (193+ 941)		1134.00
Floor Area Gross Total		8643.00 Sq.Ft.

FLOOR AREA CALCULATIONS
2ND FLOOR- MAIN RESIDENCE
Scale 1/16"= 1'-0"

LIVING AREA (Conditioned)		
Area	Size	Area (Sq.Ft.)
A	36'-6"x 79'-6"	2902.00
B	6'-0"x 20'-7"	123.00
C	23'-5"x 51'-8"	1213.00
D	26'-11"x 24'-11"	670.00
E	21'-2"x 14'-7"	308.00
F	26'-11"x 17'-5"	468.00
Total		5684.00
Floor Area Gross Total		5684.00 Sq.Ft.



FIRE ACCESS/ 200' REACH ANALYSIS
SCALE 1/16" = 1'-0"



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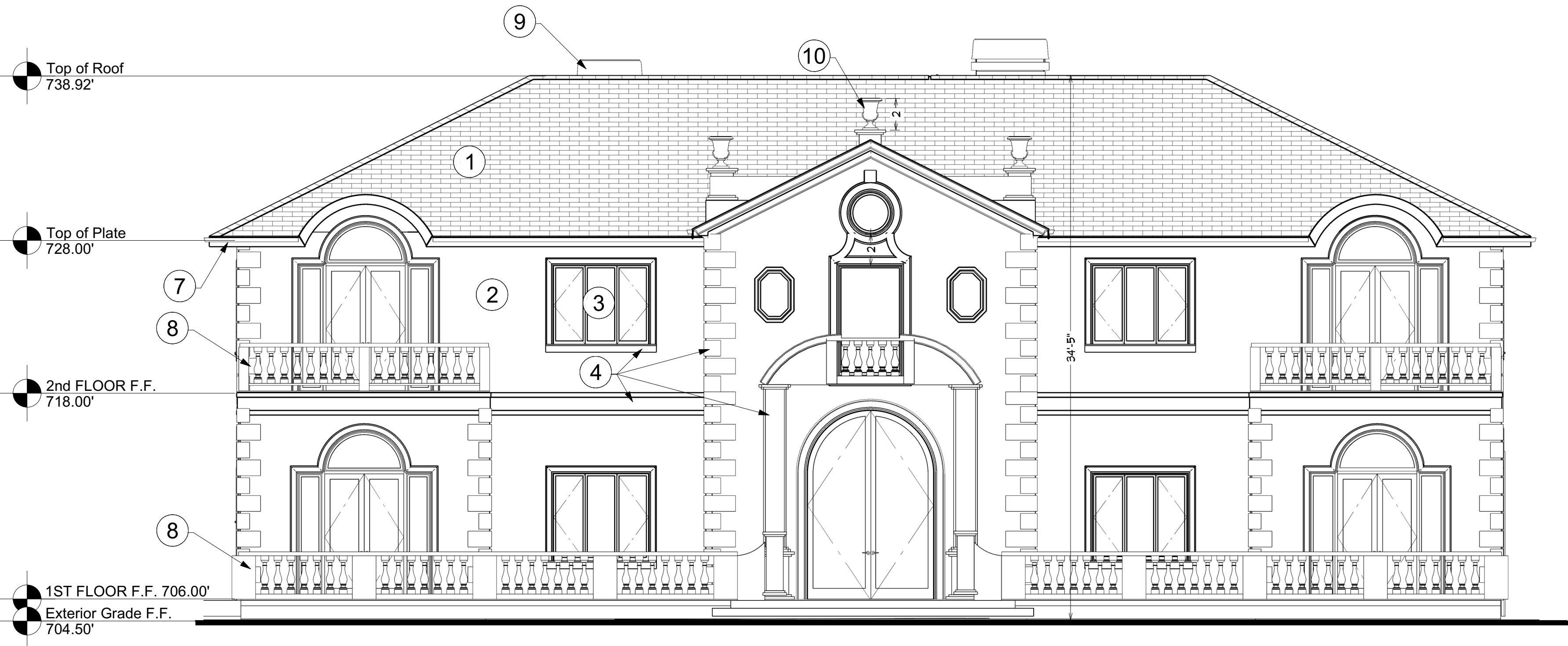
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FLOOR AREA CALCULATIONS;
CALFIRE NOTES; FIRE
ACCESS 200' REACH
ANALYSIS

PROJECT NO:
2021-04-13

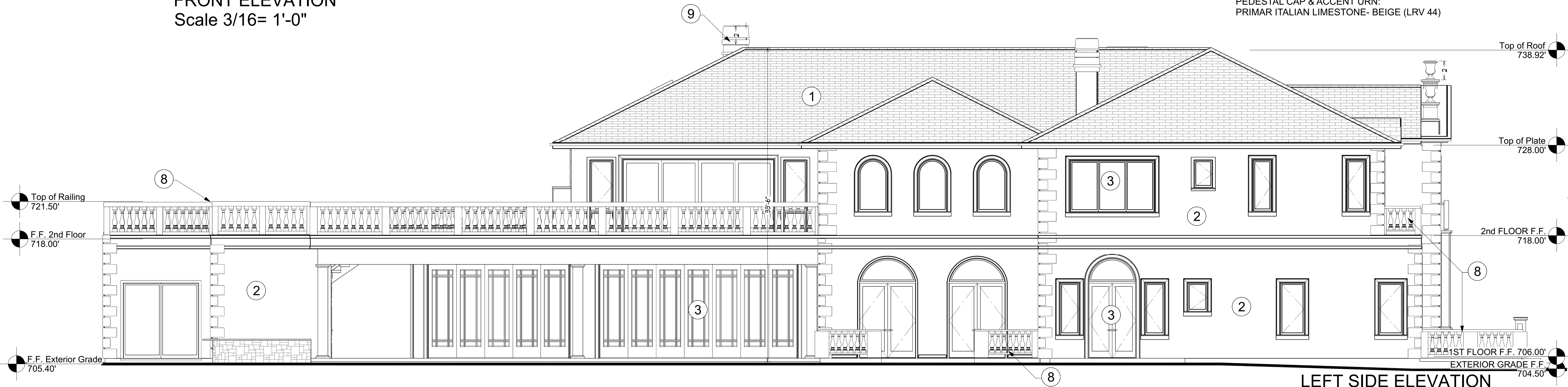
DRAWING NO:

A 2.3

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FRONT ELEVATION
Scale 3/16= 1'-0"



LEFT SIDE ELEVATION
Scale 3/16"= 1'-0"



RIGHT SIDE ELEVATION
Scale 3/16"= 1'-0"

LEGEND

- 1 ROOF: SLATE (Min. Class 'B')
Color: GREY/ GREEN Semi-Weathering (LRV 19)
by VERMONT SLATE CO.
- 2 EXTERIOR WALLS:
Cement plaster or Stucco Painted (Smooth Finish)
COLOR- Benjamin Moore # CSP 375 (LRV41.21)
- 3 WINDOWS/ DOORS:
Type: METAL, Anderson E Series Aluminum Exterior
COLOR: URBAN BRONZE (LRV 10)
- 4 DOOR/ WINDOW TRIMS & ACCENT MOLDINGS:
PRIMAR ITALIAN LIMESTONE
COLOR- BEIGE Smooth Honed (LRV 44)
- 5 FRONT ENTRANCE WALL:
PRIMAR ITALIAN LIMESTONE
COLOR: BEIGE Smooth Honed (LRV 44)
- 6 FRONT ENTRANCE DOOR:
METAL
COLOR- BLACK
- 7 GUTTERS & DOWNSPOUTS:
COPPER (LRV 16.79)
- 8 RAILINGS:
PRIMAR ITALIAN LIMESTONE
COLOR: BEIGE Smooth Honed (LRV 44)
- 9 CHIMNEY:
WALL- STUCCO (EXTERIOR WALL COLOR & FINISH, LRV 41.21)
ACCENT TRIM @ TOP- PRIMAR ITALIAN LIMESTONE- BEIGE (LRV 44)
CAP- COPPER CAP (LRV 16.79)
- 10 DECORATIVE ACCENT URNS @ ROOF TOP: (2' HT.)
PEDESTAL BASE: STUCCO (EXTERIOR WALL FINISH, LRV 41.21)
PEDESTAL CAP & ACCENT URN:
PRIMAR ITALIAN LIMESTONE- BEIGE (LRV 44)



YOGESH CHITLANGIA
Architect/ Designer
332 Torino Drive, #3, SAN CARLOS,
CA 94070

TEL: (650)-560-5550
FAX: (650)-560-5551
YOGESH@AADIARCH.COM

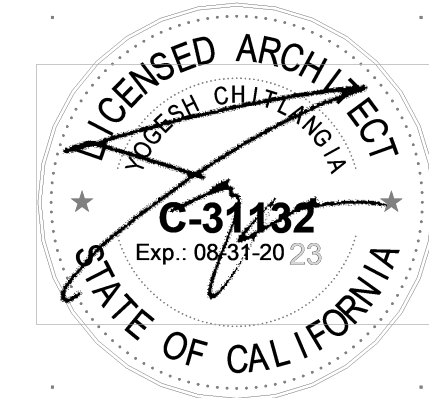
Art Designer, Inc.
NADER GAWARGY

LOS ALTOS, CA 94024
Tel. 650-948-8492

NEW RESIDENCE & ADU

15581 GLEN UNA DRIVE,
LOS GATOS,
CA 95030
APN# 510-026-007

Date	Issued For
07.01.22	PLANNING
11.14.22	PLANNING Rev 1



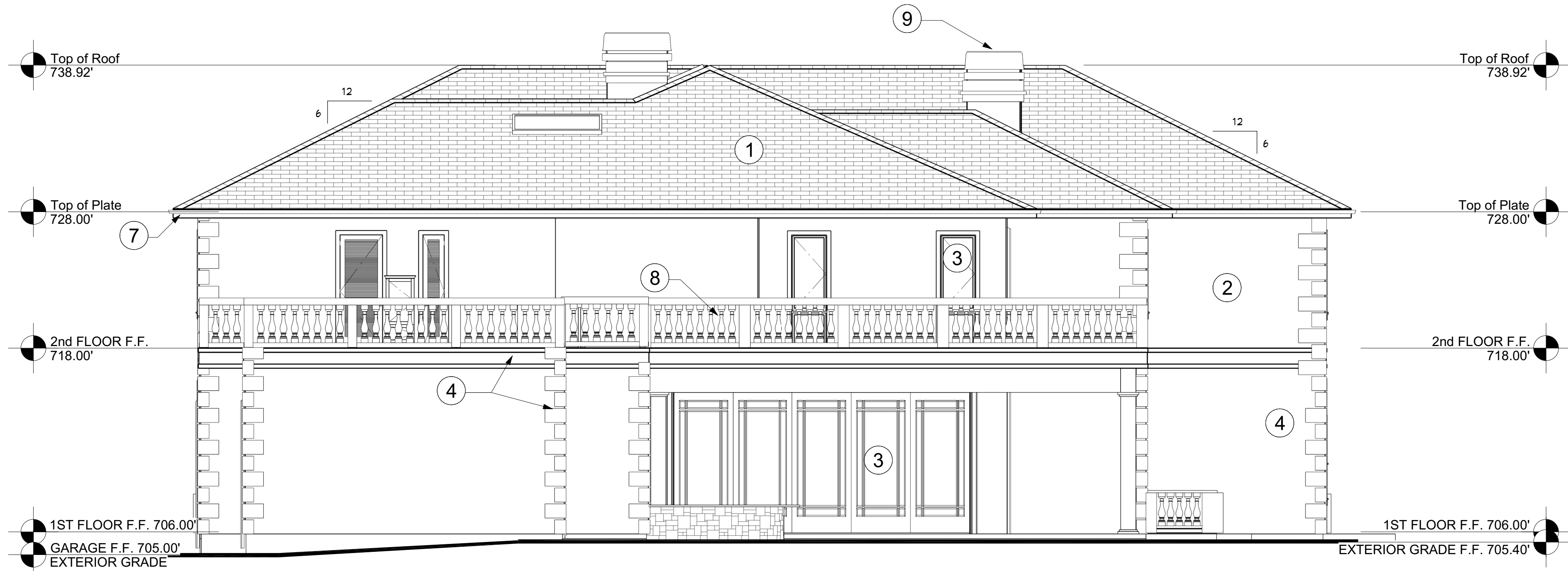
TITLE:
MAIN RESIDENCE
PROPOSED ELEVATIONS

PROJECT NO:
2021-04-13

DRAWING NO:

A 3.0

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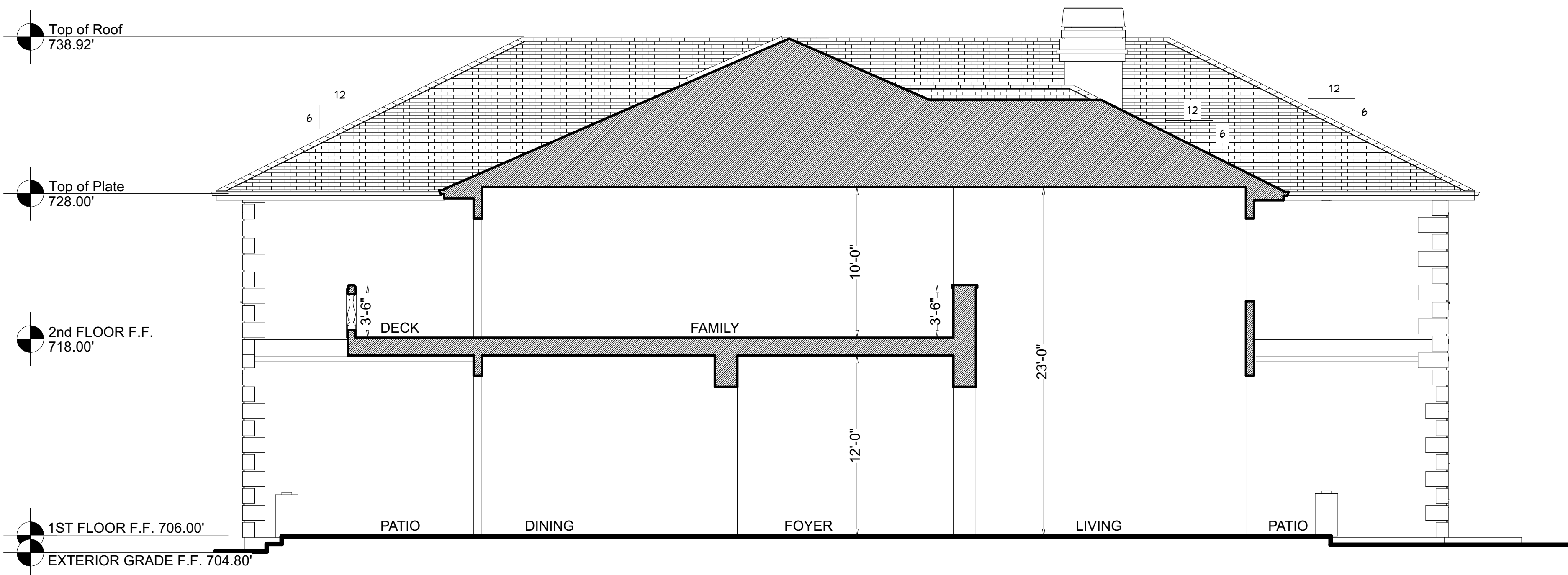


REAR ELEVATION
Scale 3/16"= 1'-0"

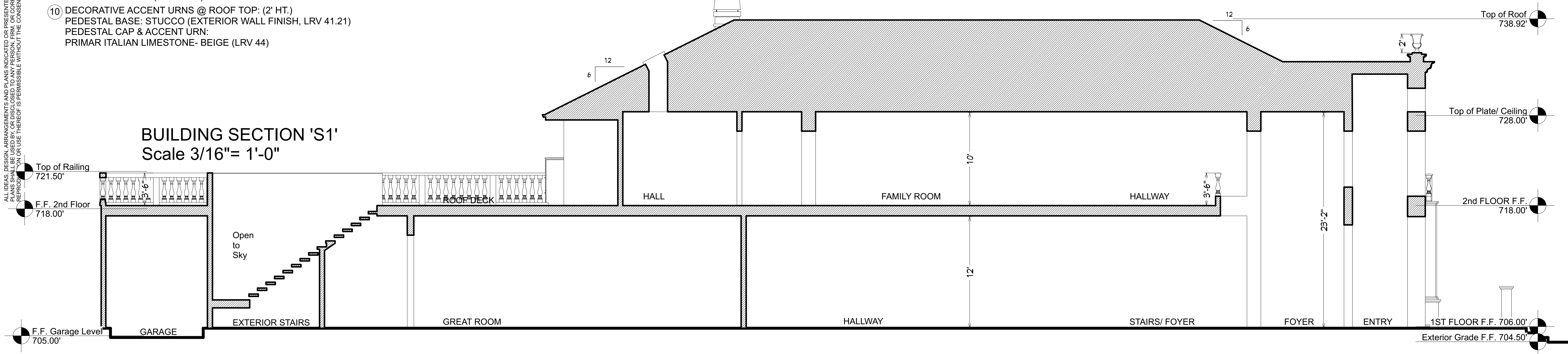
Building Section 'S2'
Scale 3/16"= 1'-0"

LEGEND

- 1 ROOF: SLATE (Min. Class 'B')
Color: GREY/ GREEN Semi-Weathering (LRV 19)
by VERMONT SLATE CO.
- 2 EXTERIOR WALLS:
Cement plaster or Stucco Painted (Smooth Finish)
COLOR- Benjamin Moore # CSP 375 (LRV41.21)
- 3 WINDOWS/ DOORS:
Type: METAL, Anderson E Series Aluminum Exterior
COLOR: URBAN BRONZE (LRV 10)
- 4 DOOR/ WINDOW TRIMS & ACCENT MOLDINGS:
PRIMAR ITALIAN LIMESTONE
COLOR- BEIGE Smooth Honed (LRV 44)
- 5 FRONT ENTRANCE WALL:
PRIMAR ITALIAN LIMESTONE
COLOR: BEIGE Smooth Honed (LRV 44)
- 6 FRONT ENTRANCE DOOR:
METAL
COLOR- BLACK
- 7 GUTTERS & DOWNSPOUTS:
COPPER (LRV 16.79)
- 8 RAILINGS:
PRIMAR ITALIAN LIMESTONE
COLOR: BEIGE Smooth Honed (LRV 44)
- 9 CHIMNEY:
WALL- STUCCO (EXTERIOR WALL COLOR & FINISH, LRV 41.21)
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- 10 DECORATIVE ACCENT URNS @ ROOF TOP: (2' HT.)
PEDESTAL BASE: STUCCO (EXTERIOR WALL FINISH, LRV 41.21)
PEDESTAL CAP & ACCENT URN:
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BUILDING SECTION 'S1'
Scale 3/16"= 1'-0"



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Architect/ Designer
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CA 94070

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LOS ALTOS, CA 94024
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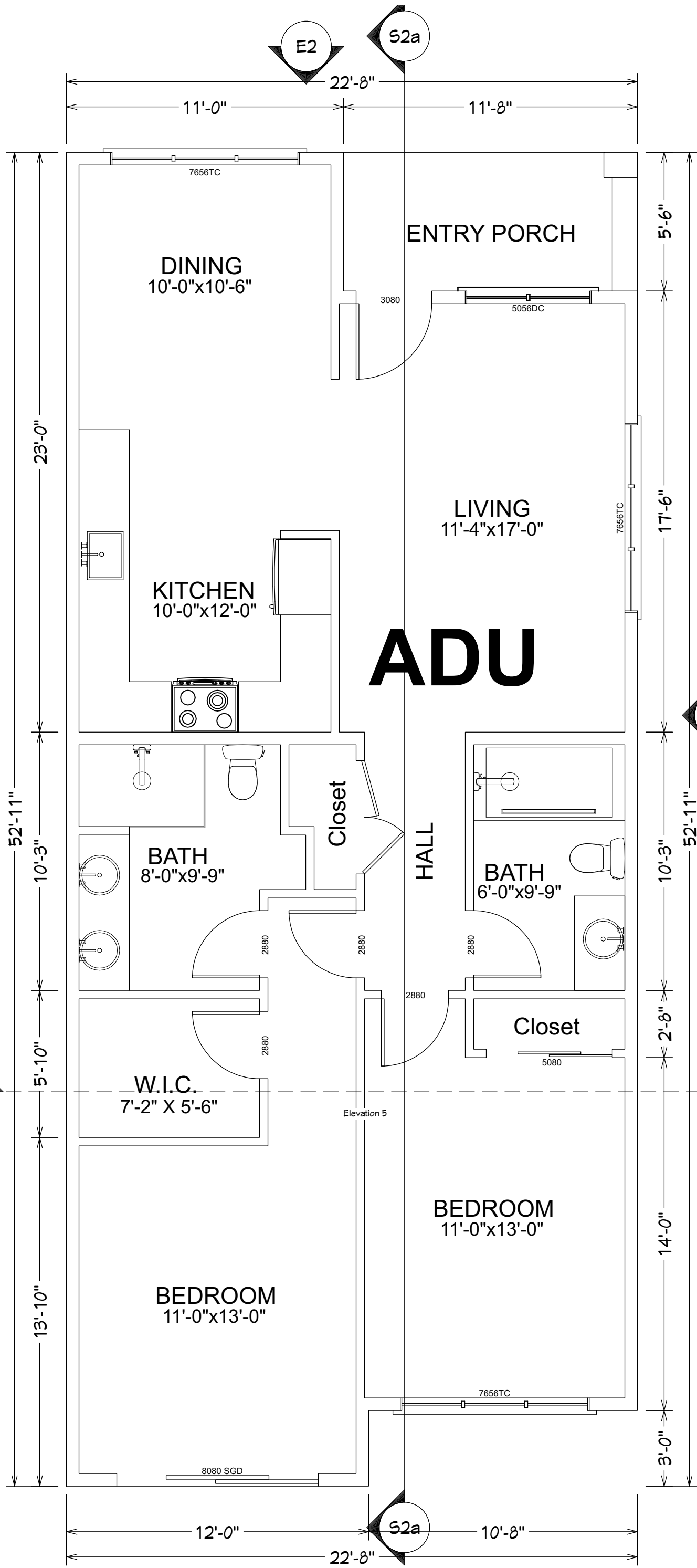
TITLE:
MAIN RESIDENCE
PROPOSED BUILDING
SECTIONS

PROJECT NO:
2021-04-13

DRAWING NO:

A 4.0

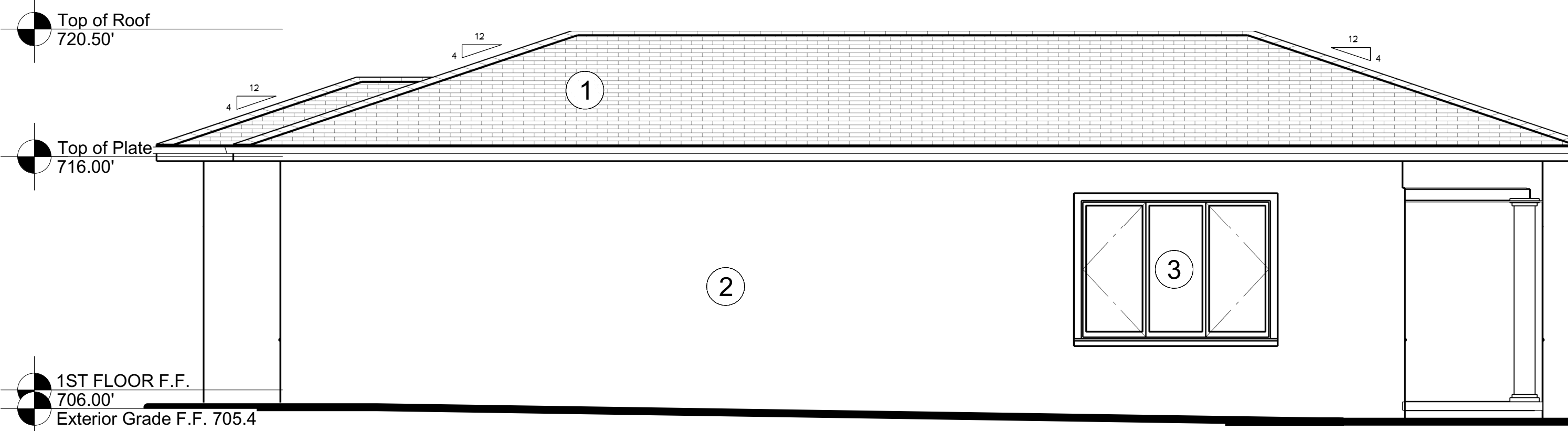
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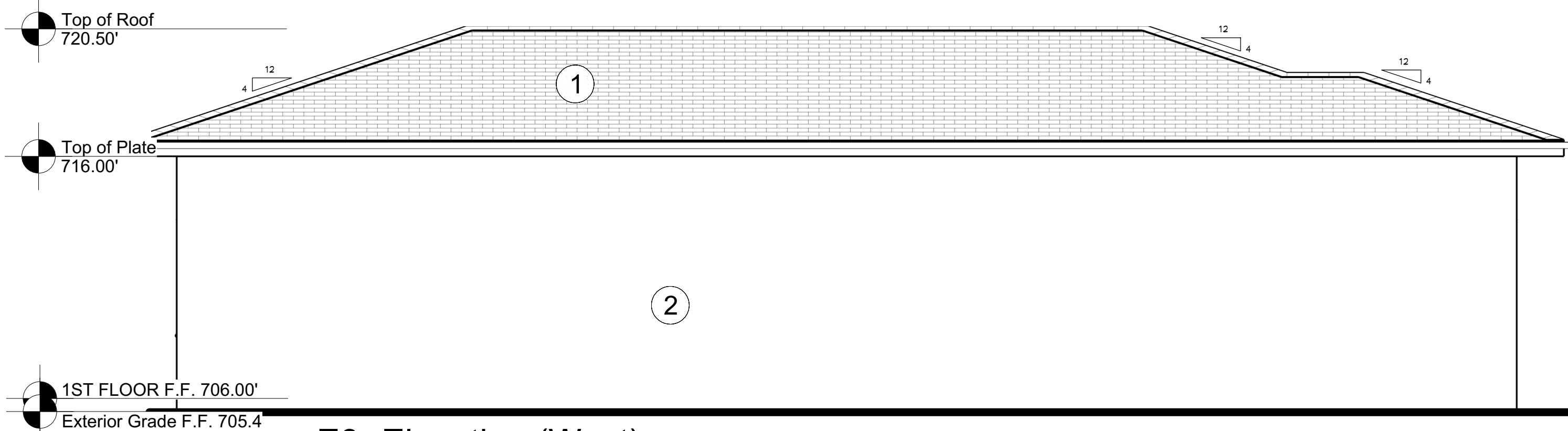
FLOOR PLAN
Scale 1/4"= 1'-0"

LEGEND

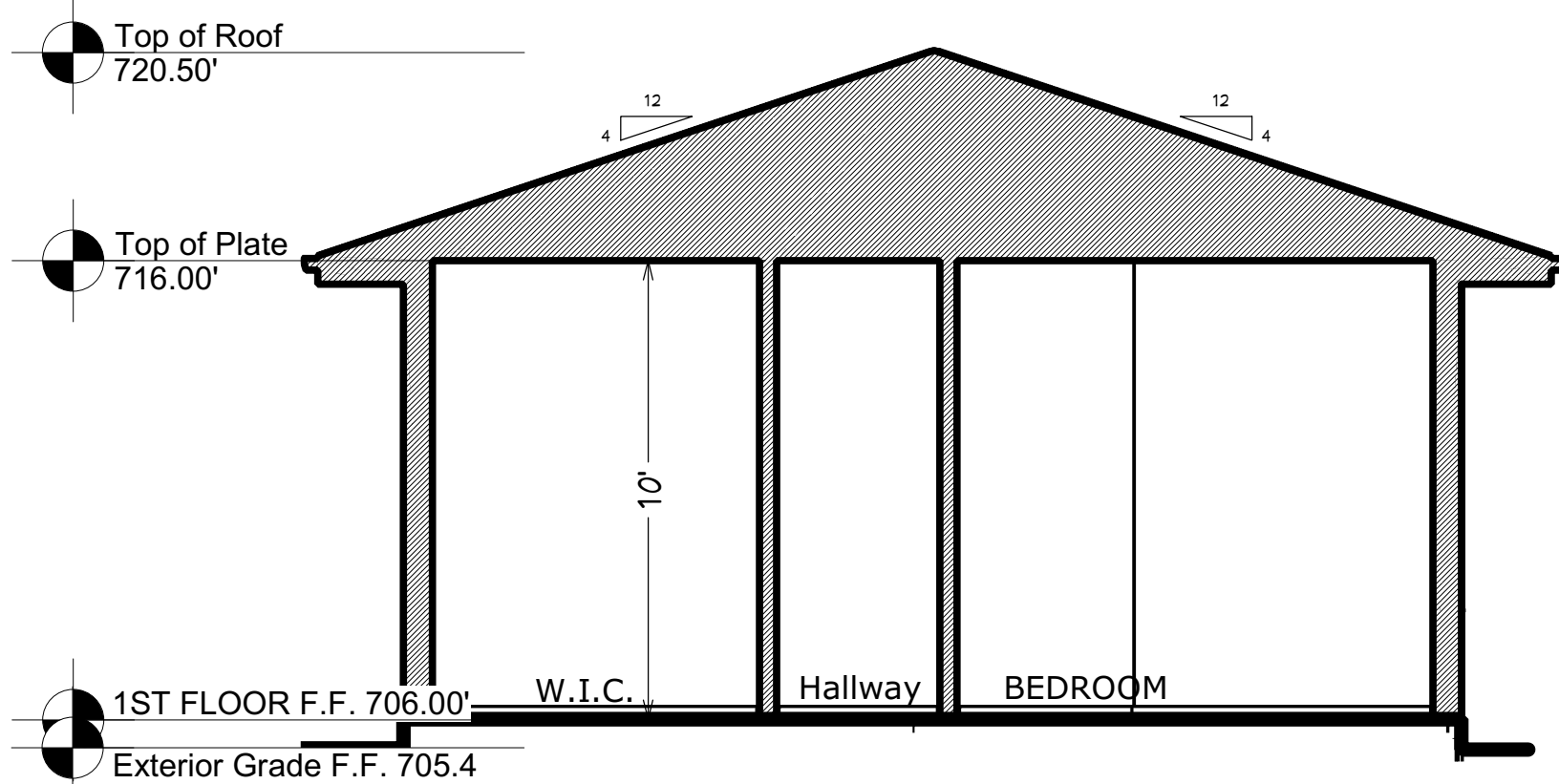
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Cement plaster or Stucco Painted (Smooth Finish)
COLOR- Benjamin Moore # CSP 375 (LRV41.21)
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COLOR- BEIGE Smooth Honed (LRV 44)
- 5 FRONT ENTRANCE WALL:
PRIMAR ITALIAN LIMESTONE
COLOR: BEIGE Smooth Honed (LRV 44)
- 6 FRONT ENTRANCE DOOR:
METAL
COLOR- BLACK
- 7 GUTTERS & DOWNSPOUTS:
COPPER (LRV 16.79)



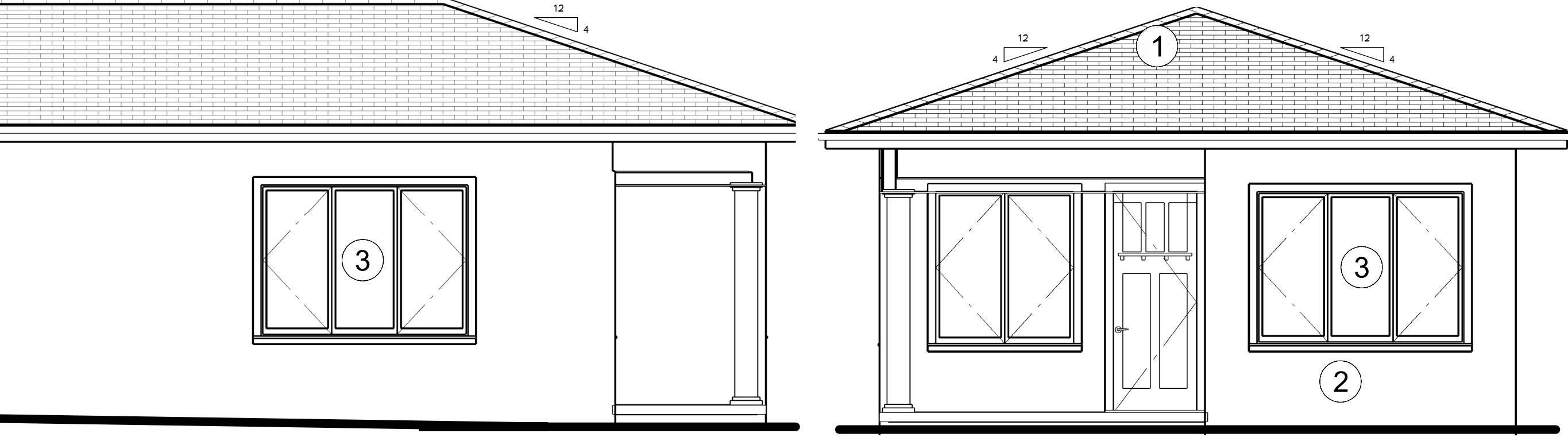
E1- Elevation (East)
Scale 1/4"= 1'-0"



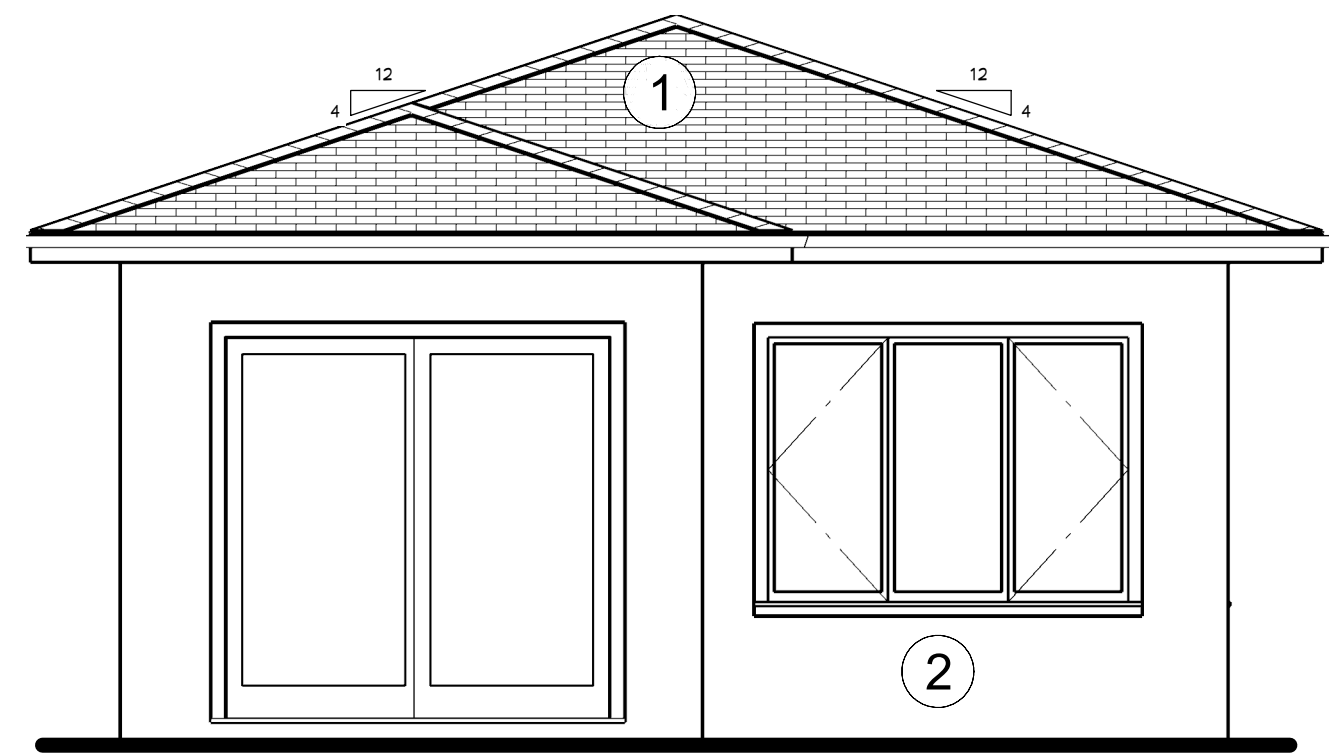
E3- Elevation (West)
Scale 1/4"= 1'-0"



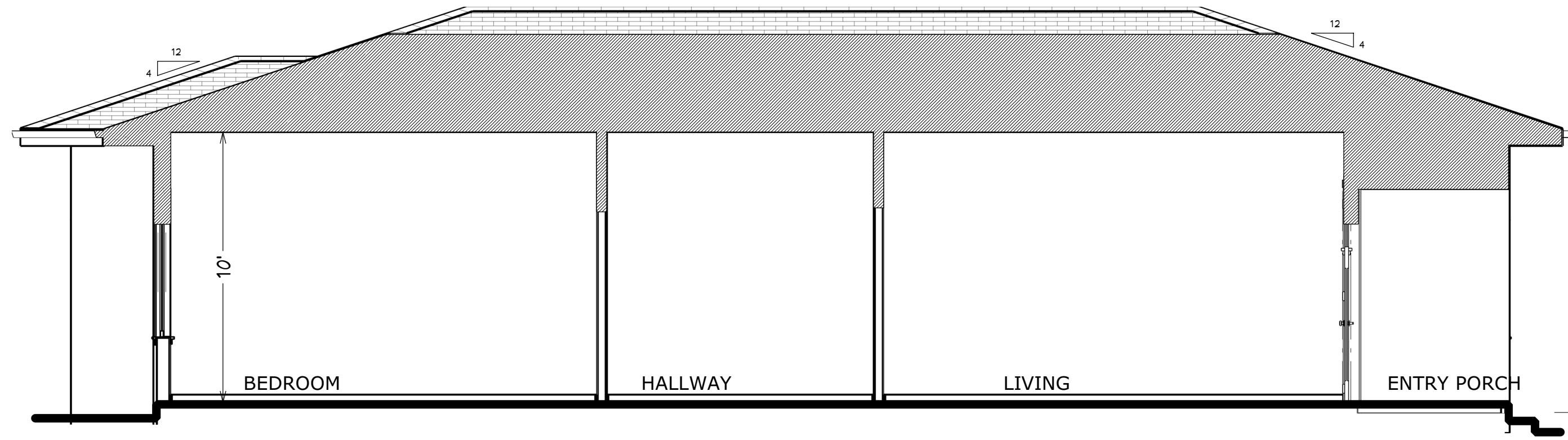
Section S1a
Scale 1/4"= 1'-0"



E2- Elevation (North)
Scale 1/4"= 1'-0"



E4- Elevation (South)
Scale 1/4"= 1'-0"



Section S2a
Scale 1/4"= 1'-0"



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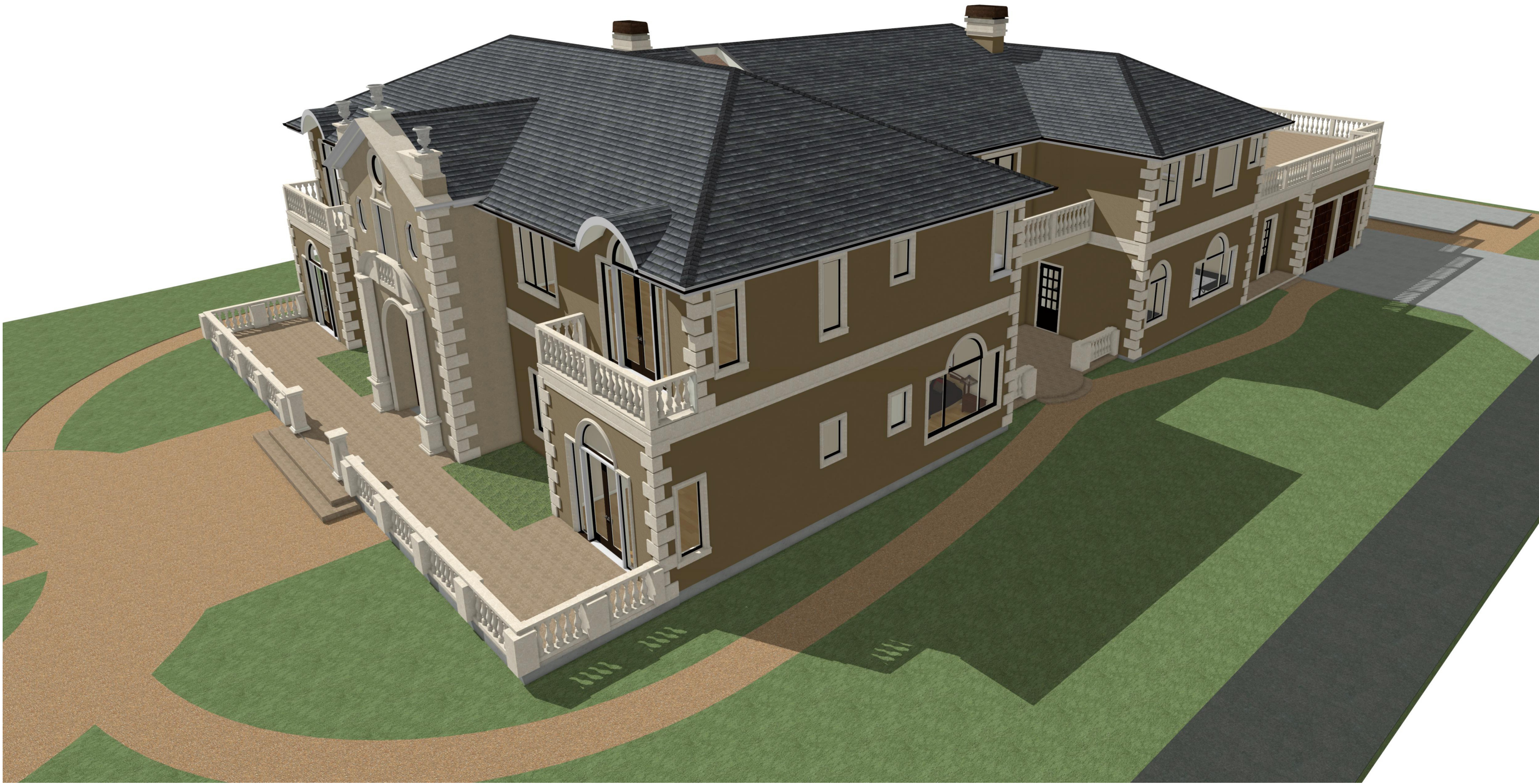
TITLE:
ACCESSORY DWELLING
UNIT
FLOOR PLAN, ELEVATIONS

PROJECT NO:
2021-04-13

DRAWING NO:

A 5.0

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332 Torino Drive, #3, SAN CARLOS,
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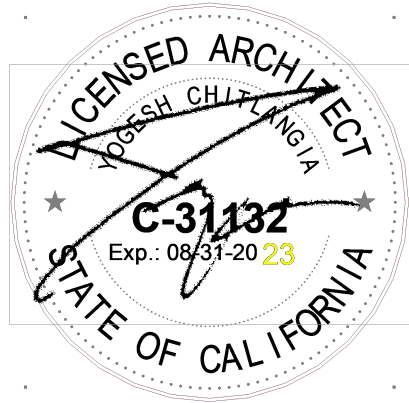
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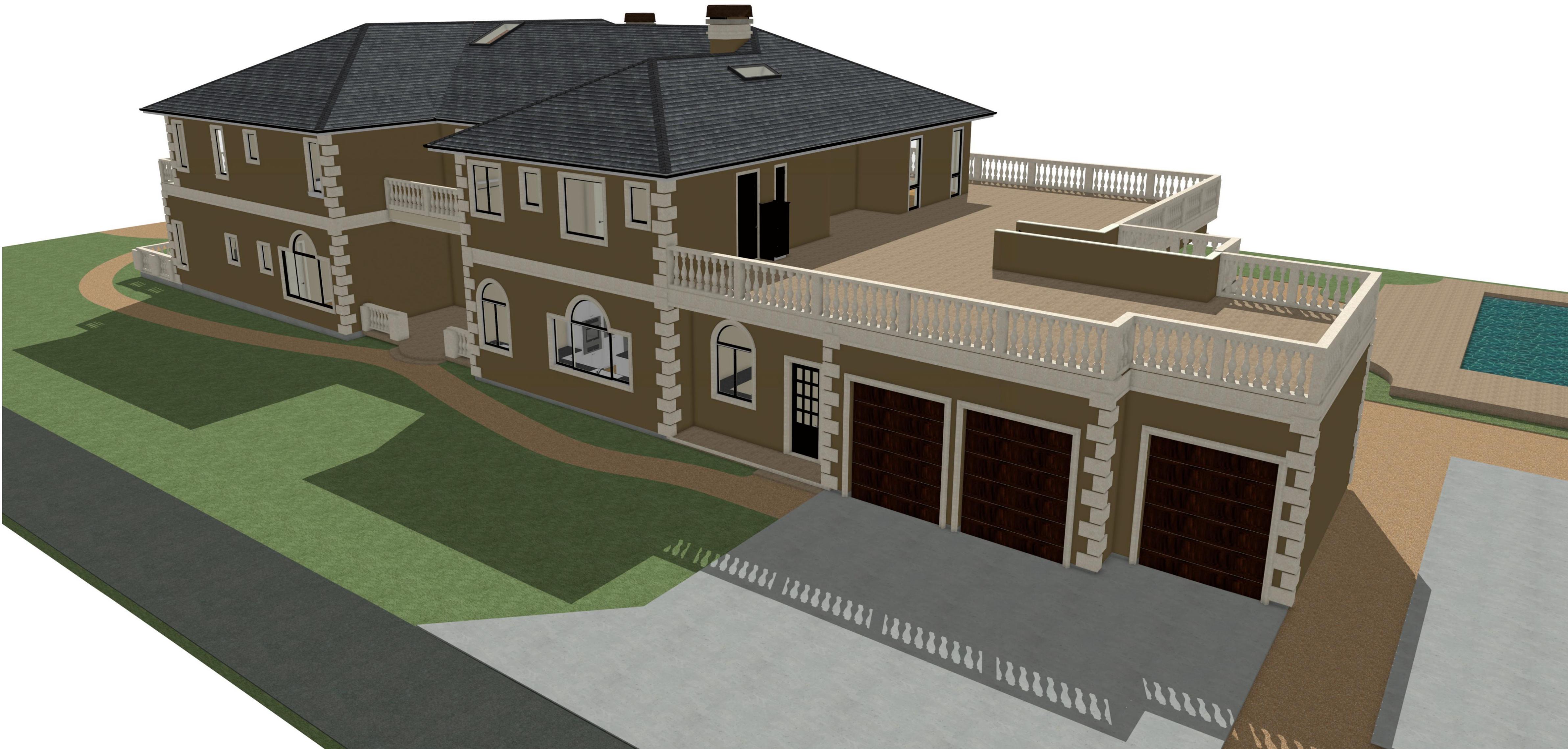
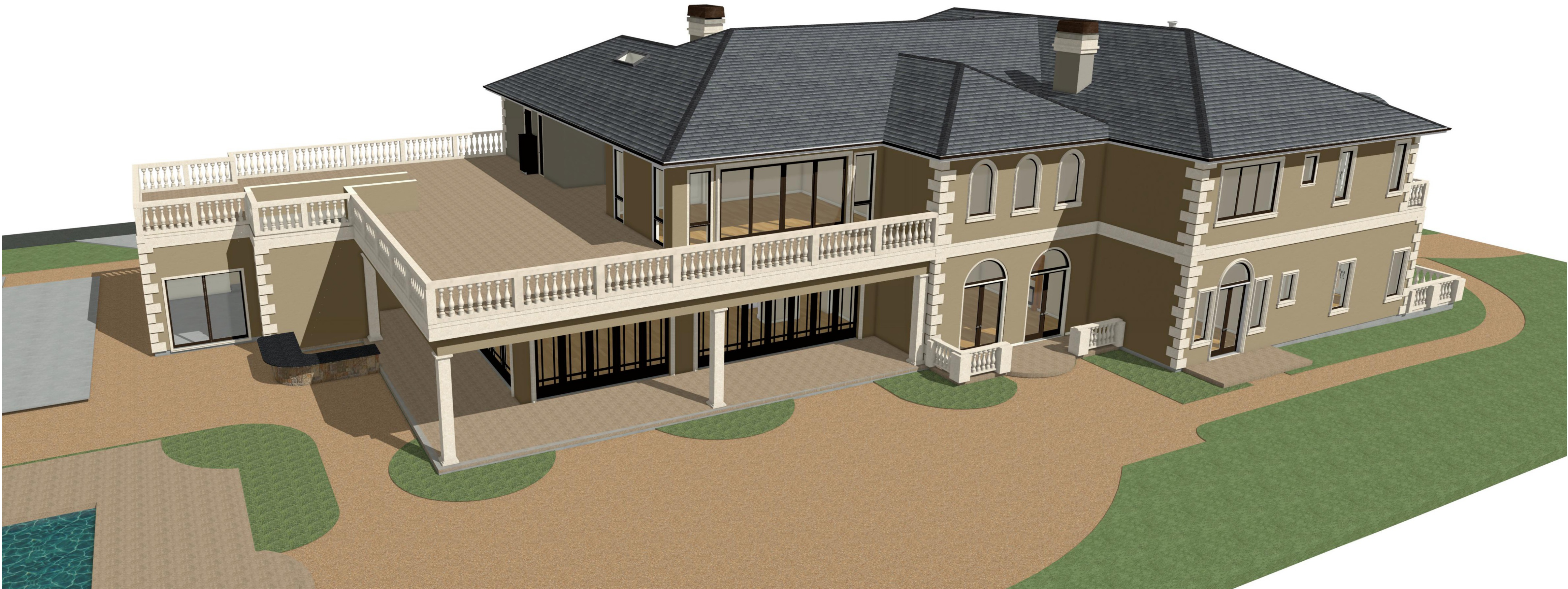
TITLE:
3D VIEWS

PROJECT NO:
2021-04-13

DRAWING NO:

A 6.0

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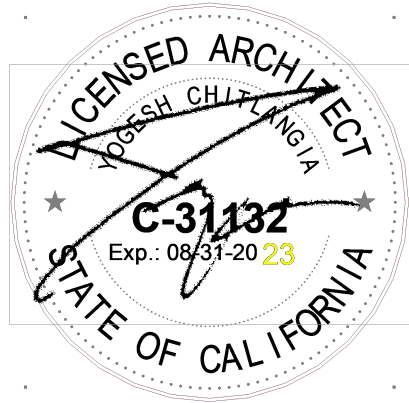
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LOS ALTOS, CA 94024
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Date	Issued For
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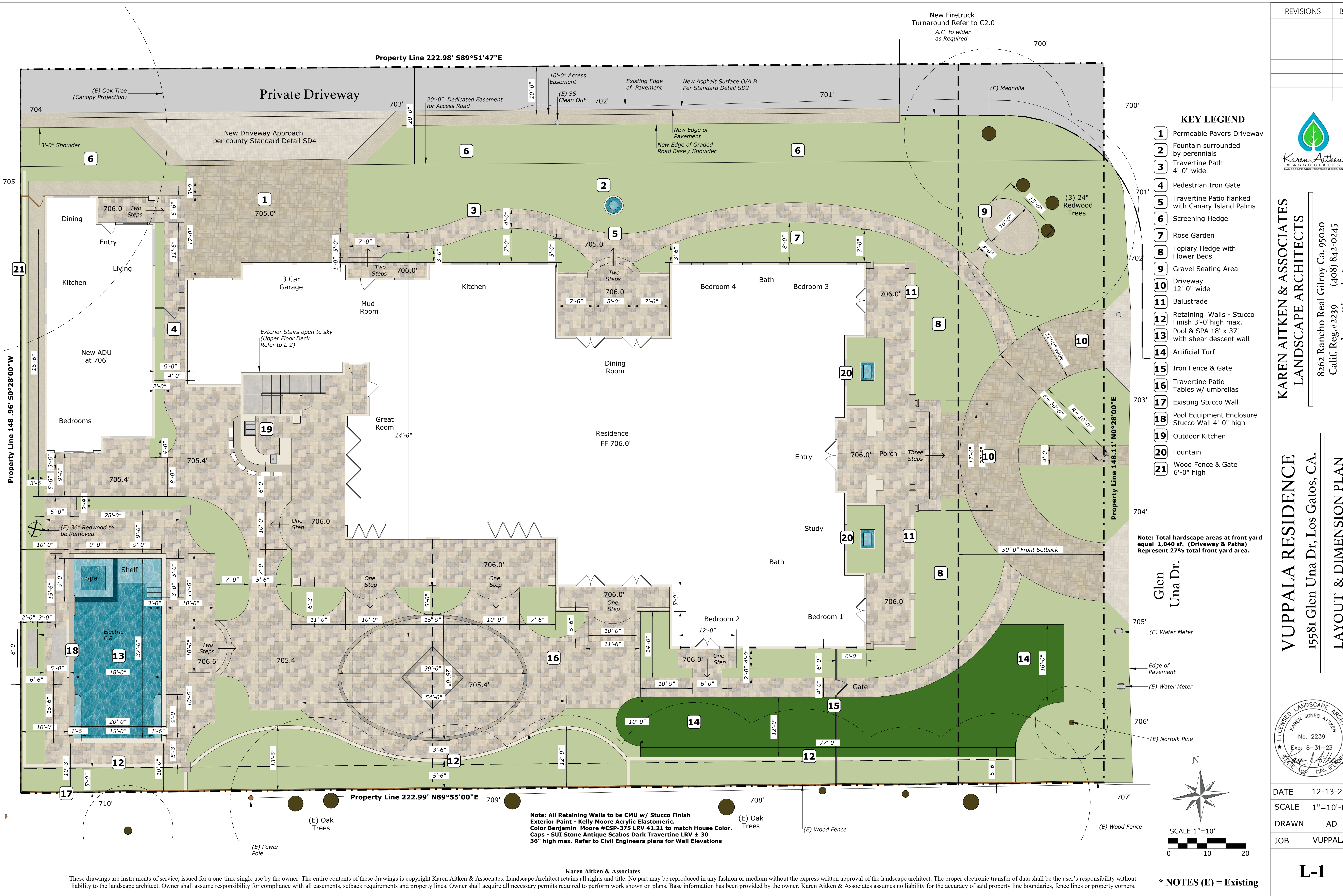


TITLE:
3D VIEWS

PROJECT NO:
2021-04-13


DRAWING NO:

A 7.0



REVISIONS

BY



KAREN AITKEN & ASSOCIATES

LANDSCAPE ARCHITECTS

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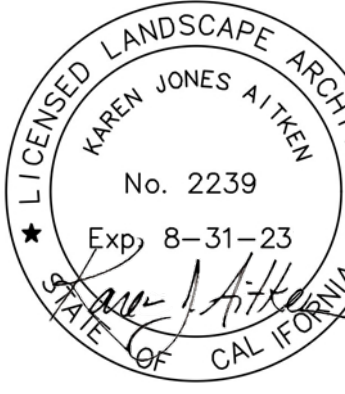
Calif. Reg.#2239 (408) 842-0245

karen@kaa.design

VUPPALA RESIDENCE

15581 Glen Una Dr, Los Gatos, CA.

LAYOUT & DIMENSION PLAN



DATE

12-13-22

SCALE

1"=10'-0"

DRAWN

AD

JOB

VUPPALA

L-1

* NOTES (E) = Existing



REVISIONS	BY

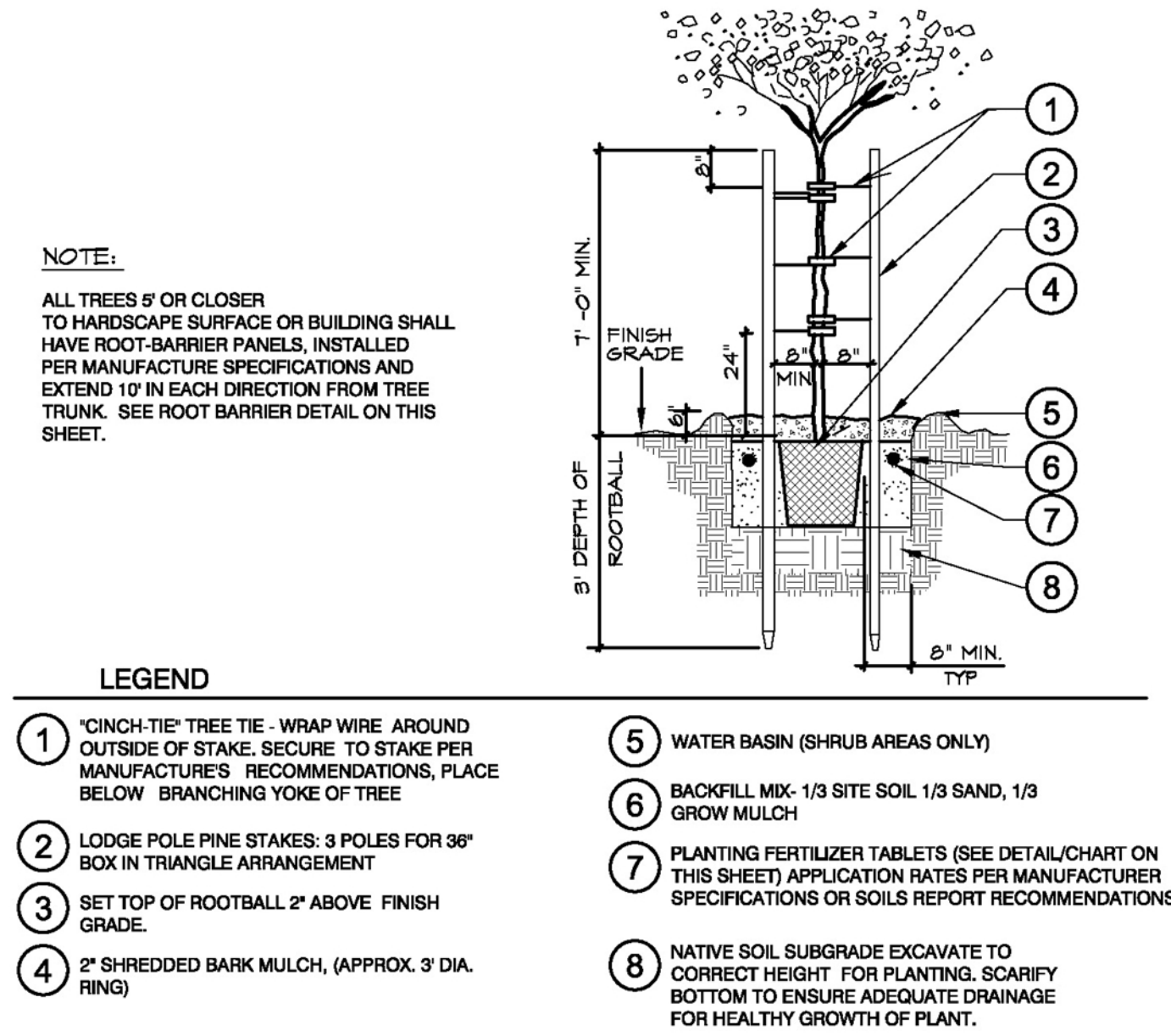


KAREN AITKEN & ASSOCIATES
LANDSCAPE ARCHITECTS
8262 Rancho Real Gilroy Ca. 95020
Calif. Reg.#2239 (408) 842-0245
karen@kaa.design

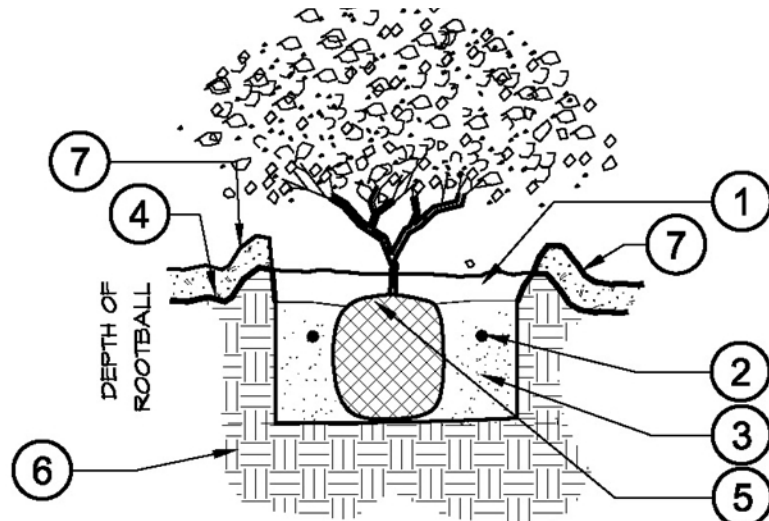
VUPPALA RESIDENCE
1558i Glen Una Dr, Los Gatos, CA.
PLANTING PLAN



DATE	12-13-22
SCALE	1"=10'-0"
DRAWN	IN
JOB	VUPPALA



TREE PLANTING WITH DOUBLE STAKE




- 1 WATER BASIN WITH 2" X 2" SHREDDED BARK MULCH.
- 2 TRI-C MYCO PAKS (SEE DETAIL "E" ON THIS SHEET). APPLICATION RATES PER MANUFACTURER SPECIFICATIONS.
- 3 BACKFILL MIX- 1/3 SITE SOIL, 1/3 SAND, 1/3 GROW MULCH.
- 4 FINISH GRADE
- 5 ROOTBALL 1"-2" ABOVE FINISH GRADE
- 6 NATIVE SOIL SUBGRADE EXCAVATE TO CORRECT HEIGHT FOR PLANTING. SCARIFY BOTTOM TO ENSURE ADEQUATE DRAINAGE FOR HEALTHY GROWTH OF PLANT.
- 7 3" MULCH LAYER

TYPICAL SHRUB PLANTING

PLANT LEGEND				
BOTANICAL	COMMON	QTY	SIZE	WATER
Tree				
Betula jacquemontii	Jacquemontii Birch	6	24" Box	Medium
Cercis occidentalis	Western Redbud	2	24" Box	Very Low
Cotinus coggygia 'Purpureus'	Purple Smoketree	1	15 Gallon	Low
Lagerstroemia indica 'Tuscarora'	Tuscarora Crape Myrtle	6	24" Box	Low
Laurus nobilis	Grecian Laurel	3	24" Box	Low
Olea europaea 'Majestic Beauty'	Majestic Beauty Fruitless Olive	2	24" Box	Low
Shrub				
Buxus 'Green Beauty'	Globe Japanese Boxwood	87	5 Gallon	Medium
Buxus microphylla japonica	Japanese Boxwood	64	5 Gallon	Medium
Camellia japonica	Japanese Camellia	6	5 Gallon	Medium
Coleonema pulchellum 'Sunset Gold'	Golden Breath Of Heaven	9	5 Gallon	Medium
Hibiscus rosa-sinensis 'Seminole'	Seminole Hibiscus	1	5 Gallon	Medium
Hydrangea macrophylla	Bigleaf Hydrangea	12	5 Gallon	High
Lavandula angustifolia 'Hidcote'	Hidcote English Lavender	12	5 Gallon	Low
Ligustrum japonicum	Japanese Privet	45	5 Gallon	Medium
Loropetalum chinense 'Rubrum'	Red Fringe Flower	13	5 Gallon	Medium
Nandina domestica	Nandina, Heavenly Bamboo	7	5 Gallon	Low
Pittosporum tenuifolium	Blackstem Pittosporum	12	5 Gallon	Medium
Prunus caroliniana 'Bright n' Tight'	Bright n' Tight Carolina Laurel	22	5 Gallon	Low
Prunus caroliniana 'Compacta'	Dwarf Carolina Laurel Cherry	28	5 Gallon	Low
Raphiolepis umbellata 'Minor'	White Compact Yeddo Hawthorn	23	5 Gallon	Low
Rosa 'Iceberg'	Iceberg Floribunda Rose	30	5 Gallon	Medium
Rosa Hybrid Tea varieties	Hybrid Tea Rose (selections)	45	5 Gallon	Medium
Rosmarinus officinalis	Rosemary	12	5 Gallon	Low
Verbena 'Homestead Purple'	Homestead Purple Verbena	10	1 Gallon	Low
Ground cover				
Gazania hybrids	Hybrid Gazanias	10	1 Gallon	Medium
Geranium x cantabrigiense 'Biokovo'	Biokovo Geranium	12	1 Gallon	Low
Iberis sempervirens 'Little Gem'	Little Gem Evergreen Candytuft	10	1 Gallon	Low
Lantana montevidensis	Trailing Lantana	13	5 Gallon	Low
Rosa Flower Carpet Pink	Pink Carpet Rose	16	5 Gallon	Medium
Perennial				
Alstroemeria hybrids	Peruvian Lily	9	5 Gallon	Low
Salvia 'Waverly'	Waverly Sage	17	5 Gallon	Low
Conifer				
Juniperus chinensis	Chinese Juniper	13	5 Gallon	Low
Fern				
Woodwardia fimbriata	Giant Chain Fern	12	5 Gallon	Medium
Palm				
Phoenix canariensis	Canary Island Date Palm	2	48" Box	Low

REVISIONS	BY



KAREN AITKEN & ASSOCIATES
LANDSCAPE ARCHITECTS

8262 Rancho Real Gilroy Ca. 95020
Calif. Reg.#2239 (408) 842-0245
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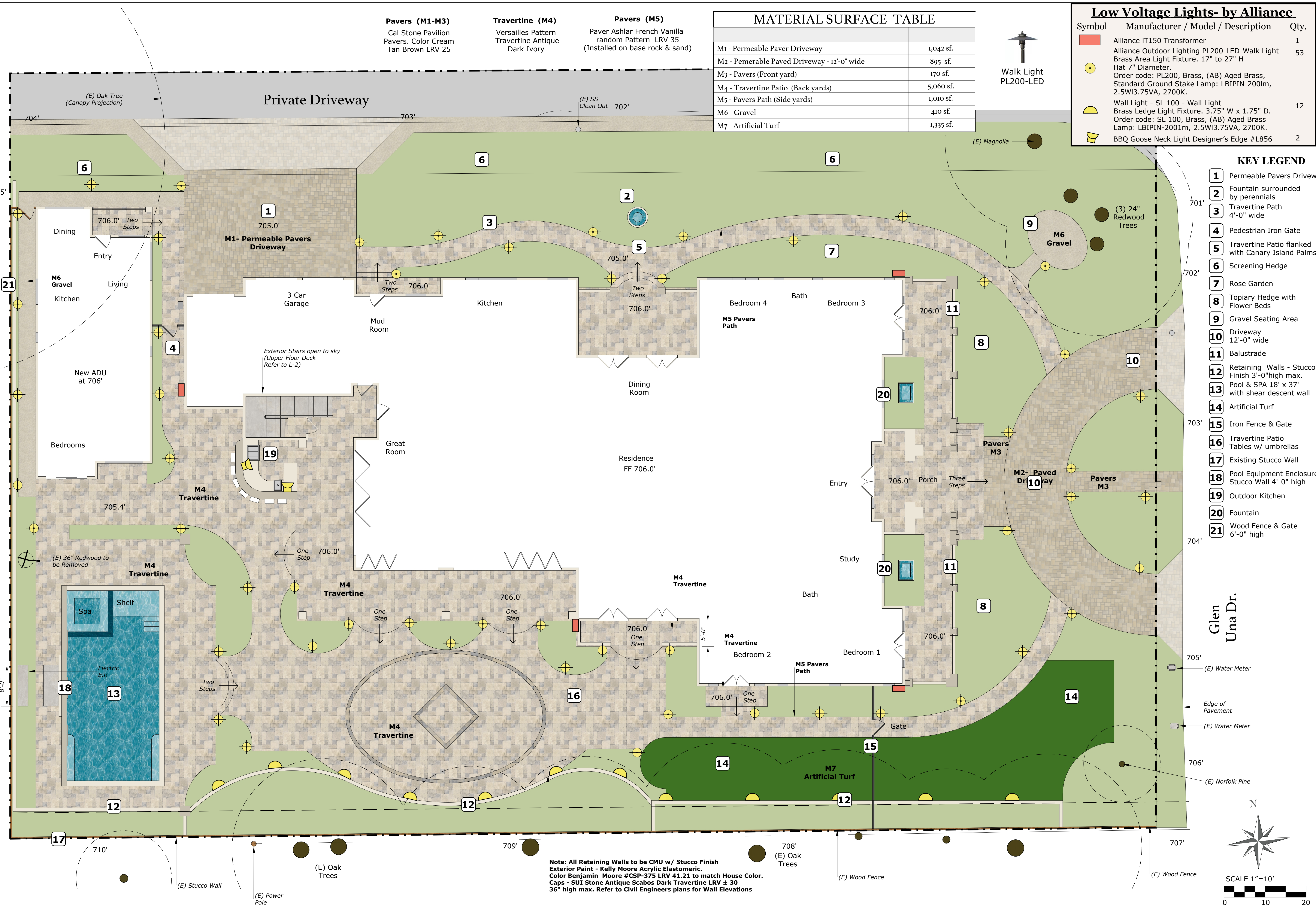
VUPPALA RESIDENCE

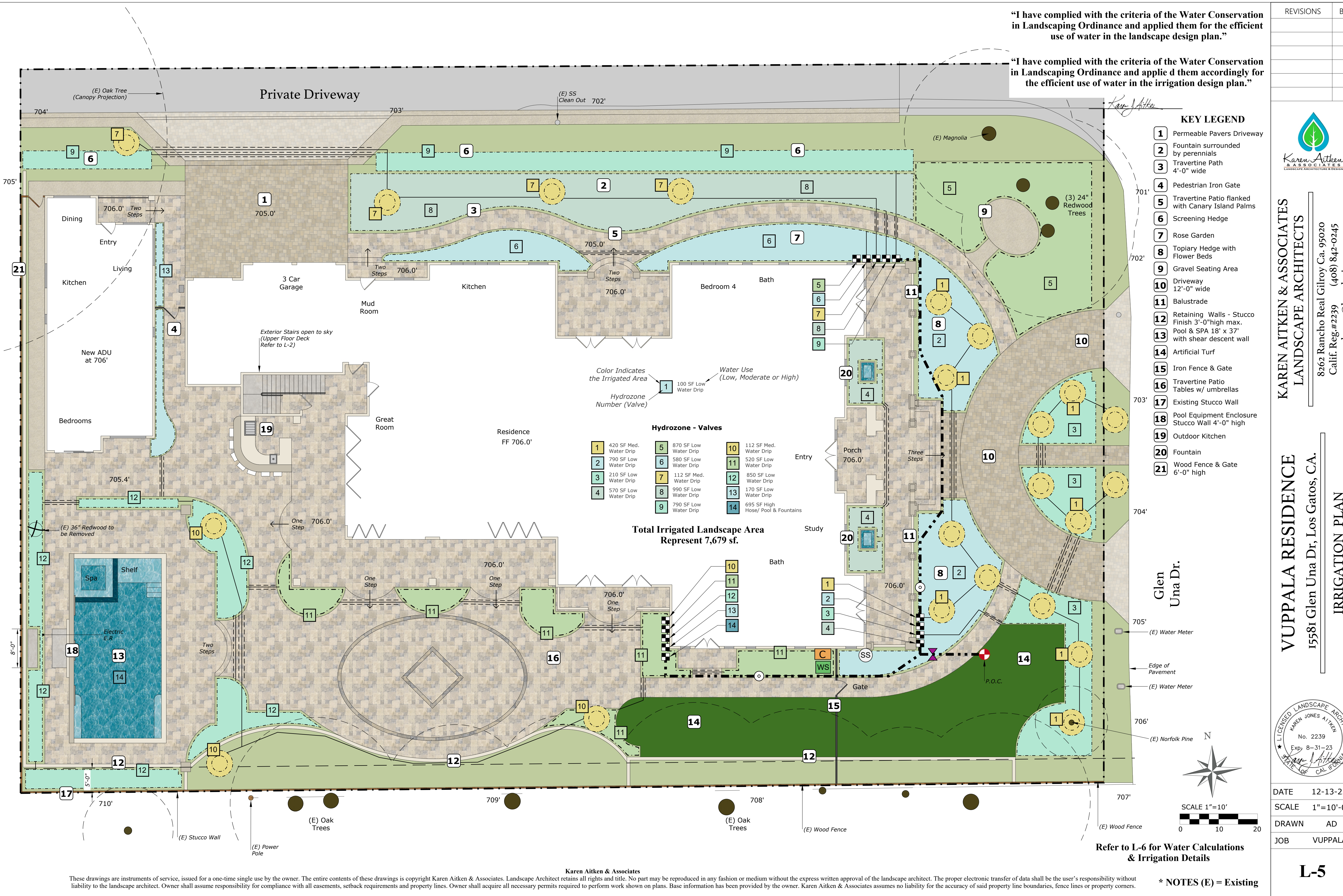
15581 Glen Una Dr, Los Gatos, CA.

PLANTING DETAILS



DATE	12-13-22
SCALE	1"=10'-0"
DRAWN	IN
JOB	VUPPALA





REVISIONS	BY



KAREN AITKEN & ASSOCIATES
LANDSCAPE ARCHITECTS
8262 Rancho Real Gilroy Ca. 95020
Calif. Reg.#2239 (408) 842-0245
karen@kaa.design

VUPPALA RESIDENCE
15581 Glen Una Dr, Los Gatos, CA.
IRRIGATION PLAN



DATE	12-13-22
SCALE	1"=10'-0"
DRAWN	AD
JOB	VUPPALA

L-5

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Calif. Reg.#2239 (408) 842-0245
karen@kaa.design

WATER CALCULATIONS & IRRIGATION DETAILS



JOB	VUPPALA
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COUNTY OF SANTA CLARA

General Construction Specifications

GENERAL CONDITIONS

1. ALL CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SOILS AND/OR GEOTECHNICAL REPORT PREPARED BY _____ DATED _____ THIS REPORT IS SUPPLEMENTED BY: 1) THESE PLANS AND SPECIFICATIONS, 2) THE COUNTY OF SANTA CLARA STANDARD DETAILS, 3) THE COUNTY OF SANTA CLARA STANDARD SPECS, 4) STATE OF CALIFORNIA STANDARD DETAILS, 5) STATE OF CALIFORNIA STANDARD SPECIFICATIONS, IN THE EVENT OF CONFLICT THE FORMER SHALL TAKE PRECEDENCE OVER THE LATTER. THE PERFORMANCE AND COMPLETION OF ALL WORK MUST BE TO THE SATISFACTION OF THE COUNTY.
2. DEVELOPER IS RESPONSIBLE FOR INSTALLATION OF THE IMPROVEMENTS SHOWN ON THESE PLANS AND HE OR HIS SUCCESSOR PROPERTY OWNERS ARE RESPONSIBLE FOR THEIR CONTINUED MAINTENANCE.
3. DEVELOPER SHALL BE RESPONSIBLE FOR CORRECTION OF ANY ERRORS OR OMISSIONS IN THESE PLANS. THE COUNTY SHALL BE AUTHORIZED TO REQUIRE DISCONTINUANCE OF ANY WORK AND SUCH CORRECTION AND MODIFICATION OF PLANS AS MAY BE NECESSARY TO COMPLY WITH COUNTY STANDARDS OR CONDITIONS OF DEVELOPMENT APPROVAL.
4. DEVELOPER SHALL OBTAIN ENCROACHMENT PERMITS FROM THE SANTA CLARA VALLEY WATER DISTRICT AND CALIFORNIA DEPARTMENT OF TRANSPORTATION WHERE NEEDED, COPIES OF THESE PERMITS SHALL BE KEPT AT THE JOB SITE FOR REVIEW BY THE COUNTY'S INSPECTOR.
5. DEVELOPER SHALL REMOVE OR TRIM ALL TREES TO PROVIDE AN UNOBSTRUCTED FIFTEEN (15) FOOT VERTICAL CLEARANCE FOR ROADWAY AREA. THIS PLAN AUTHORIZES THE REMOVAL OF ONLY THOSE TREES WITH TRUNK DIAMETERS GREATER THAN 12 INCHES MEASURED 4.5 FEET ABOVE THE GROUND THAT ARE SHOWN TO BE REMOVED UNLESS AN AMENDED PLAN IS APPROVED OR A SEPARATE TREE REMOVAL PERMIT IS OBTAINED FROM THE PLANNING OFFICE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT REMOVAL OF ADDITIONAL TREES HAS BEEN PERMITTED.
7. DEVELOPER SHALL PROVIDE ADEQUATE DUST CONTROL AS REQUIRED BY THE COUNTY INSPECTOR.
8. ALL PERSONS MUST COMPLY WITH SECTION 4442 OF THE PUBLIC RESOURCES CODE AND SECTION 13005 OF THE HEALTH AND SAFETY CODE RELATING TO THE USE OF SPARK ARRESTERS.
9. UPON DISCOVERING OR UNEARTHING ANY BURIAL SITE AS EVIDENCED BY HUMAN SKELETAL REMAINS OR ARTIFACTS, THE PERSON MAKING SUCH DISCOVERY SHALL IMMEDIATELY NOTIFY THE COUNTY CORNER AT (4008) 454-2520 AND LAND DEVELOPMENT ENGINEERING OFFICE AT (408) 299-5730. NO FURTHER DISTURBANCE OF THE SITE MAY BE MADE EXCEPT AS AUTHORIZED BY THE LAND DEVELOPMENT OFFICE IN ACCORD WITH PROVISIONS OF THIS ORDINANCE (COUNTY ORDINANCE CODE SECTION BE-15).
10. THESE PLANS ARE FOR THE WORK DESCRIBED IN THE SCOPE OF WORK ONLY. A SEPARATE PERMIT WILL BE REQUIRED FOR THE SEPTIC LINE CONSTRUCTION.
11. ANY DEVIATION FROM THESE APPROVED PLANS SHALL BE RE-APPROVED IN WRITING BY THE COUNTY ENGINEER PRIOR TO CONSTRUCTION.

CONSTRUCTION STAKING

1. THE DEVELOPER'S ENGINEER IS RESPONSIBLE FOR THE INITIAL PLACEMENT AND REPLACEMENT OF CONSTRUCTION GRADE STAKES. THE STAKES ARE TO BE ADEQUATELY IDENTIFIED, LOCATED, STABILIZED, ETC. FOR THE CONVENIENCE OF CONTRACTORS. LATERAL OFFSET OF STAKES SET FOR CURBS AND GUTTERS SHALL NOT EXCEED 2 1/2 FEET FROM BACK OF CURB.
2. ANY PROPERTY LINE STAKES OR ROAD MONUMENTS DISTURBED DURING CONSTRUCTION SHALL BE REPLACED BY DEVELOPER'S ENGINEER AND LICENSED LAND SURVEYOR.
3. PROPERTY LINE STAKING MUST BE PERFORMED BY THE PROJECT ENGINEER OR LAND SURVEYOR TO ESTABLISH OR RE-ESTABLISH THE PROJECT BOUNDARY AND SHALL BE INSPECTED BY THE COUNTY INSPECTOR PRIOR TO THE BEGINNING OF THE WORK.
4. PROPER CONSTRUCTION STAKES SHALL BE SET IN THE FIELD BY THE PROJECT ENGINEER OR LAND SURVEYOR AND VERIFIED BY THE COUNTY INSPECTOR PRIOR TO THE COMMENCEMENT OF GRADING.

CONSTRUCTION INSPECTION

1. CONTRACTOR SHALL NOTIFY PERMIT INSPECTION UNIT, SANTA CLARA COUNTY PRIOR TO COMMENCING WORK AND FOR FINAL INSPECTION OF WORK AND SITE. THE COUNTY REQUIRES A MINIMUM OF 24 HOURS ADVANCE NOTICE FOR GENERAL INSPECTION, 48 HOURS FOR ASPHALT CONCRETE INSPECTION.
3. INSPECTION BY SANTA CLARA COUNTY SHALL BE LIMITED TO INSPECTION OF MATERIALS AND PROCESSES OF CONSTRUCTION TO OBSERVE THERE COMPLIANCE WITH PLANS & SPECIFICATIONS BUT DOES NOT INCLUDE RESPONSIBILITY FOR THE SUPERINTENDING OF CONSTRUCTION, SITE CONDITIONS, EQUIPMENT OR PERSONNEL. CONTRACTOR SHALL NOTIFY THE COUNTY LAND DEVELOPMENT INSPECTOR AT PHONE (408) 299-8868 AT LEAST 24 HOURS PRIOR TO COMMENCING WORK AND FOR FINAL INSPECTION OF WORK AND SITE.
4. DEVELOPER AND/OR HIS AUTHORIZED REPRESENTATIVE MUST SUBMIT WRITTEN REQUEST FOR FINAL INSPECTION AND ACCEPTANCE. SAID REQUEST SHALL BE DIRECTED TO THE INSPECTION OFFICE NOTED ON THE PERMIT FORM.
5. THE CONTRACTOR SHALL PROVIDE TO THE COUNTY CONSTRUCTION INSPECTOR WITH PAD ELEVATION AND LOCATION CERTIFICATES, PREPARED BY THE PROJECT ENGINEER OR LAND SURVEYOR, PRIOR TO COMMENCEMENT OF THE BUILDING FOUNDATION.

SITE PREPARATION (CLEARING AND GRUBBING)

1. EXISTING TREES AUTHORIZED FOR REMOVAL, ROOTS, AND FOREIGN MATERIAL IN AREAS TO BE IMPROVED WILL BE REMOVED TO AN AUTHORIZED DISPOSAL SITE AS FOLLOWS:
- A) TO A MINIMUM DEPTH OF TWO FEET BELOW THE FINISHED GRADE OF PROPOSED ROADWAYS (EITHER PRIVATE OR TO BE DEDICATED TO PUBLIC USE)
- B) FROM AREAS AFFECTED BY THE PROPOSED GRADING EXCEPT WHERE NOTED ON THE PLANS.
2. IT SHALL BE THE RESPONSIBILITY OF THE DEVELOPER TO MOVE OR RELOCATE UTILITY POLES AND OTHER OBSTRUCTIONS IN THE WAY OF CONSTRUCTION.

UTILITY LOCATION, TRENCHING & BACKFILL

1. CONTRACTOR SHALL NOTIFY USA (UNDERGROUND SERVICE ALERT) AT 1-800-277-2800 A MINIMUM OF 24 HOURS BEFORE BEGINNING UNDERGROUND WORK FOR VERIFICATION OF THE LOCATION OF UNDERGROUND UTILITIES.
2. ACCURATE VERIFICATION AS TO SIZE, LOCATION, AND DEPTH OF EXISTING UNDERGROUND CONDUITS OR FACILITIES SHALL BE THE INDIVIDUAL CONTRACTORS RESPONSIBILITY. PLAN LOCATIONS ARE APPROXIMATE AND FOR GENERAL INFORMATION ONLY.
3. ALL UNDERGROUND INSTALLATIONS SHALL BE IN PLACE AND THE TRENCH BACKFILLED AND COMPACTED BEFORE PLACING AGGREGATE BASE MATERIAL OR SURFACE STRUCTURES. SURFACING MAY BE DONE IF THE UTILITY COMPANY CONCERNED INDICATES BY LETTER THAT IT WILL BORE UNLESS SPECIFICALLY AUTHORIZED BY THE COUNTY. GAS AND WATER MAINS SHALL BE INSTALLED OUTSIDE THE PAVED AREAS.
4. TRENCH BACKFILL IN EXISTING PAVEMENT AREAS SHALL BE SAND MATERIAL IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STATE STANDARD SPECIFICATIONS. THE STRUCTURAL SECTION FOR TRENCH REPLACEMENT SHALL CONSIST OF NOT LESS THAN 12 INCHES OF APPROVED AGGREGATE BASE MATERIAL COMPACTED TO A RELATIVE COMPACTION OF AT LEAST 95% AND 4 INCHES OF HOT ASPHALT CONCRETE PLACED IN TWO LIFTS. TRENCH RESTORATION FOR HIGHER TYPE PAVEMENTS SHALL BE MADE IN KIND OR AS DIRECTED BY THE COUNTY.
5. TRENCH BACKFILL IN NEW CONSTRUCTION AREAS SHALL BE SAND MATERIAL COMPACTED TO A RELATIVE COMPACTION OF AT LEAST 90%. THE REQUIREMENT FOR SELECT MATERIAL MAY BE WAIVED BY COUNTY IF THE NATIVE SOIL IS SUITABLE FOR USE AS TRENCH BACKFILL BUT THE COMPACTION REQUIREMENTS WILL NOT BE THEREBY WAIVED.
6. BACKFILL AND TRENCH RESTORATION REQUIREMENTS SHALL APPLY AS MINIMUM STANDARDS TO ALL UNDERGROUND FACILITIES INSTALLED BY OTHER FIRMS OR PUBLIC AGENCIES.

RETAINING WALLS

1. REINFORCED CONCRETE AND CONCRETE MASONRY UNIT RETAINING WALLS SHALL HAVE FOUNDATION AND REINFORCEMENT INSPECTED BY THE COUNTY ENGINEERING INSPECTOR AND ENGINEER OF RECORD PRIOR TO POURING THE FOUNDATION AND FORMING THE WALL.
2. SEGMENTAL BLOCK RETAINING WALLS SHALL HAVE FOUNDATION AND REINFORCEMENT INSPECTED BY THE COUNTY ENGINEERING INSPECTOR.

GRADING

1. EXCAVATED MATERIAL SHALL BE PLACED IN THE FILL AREAS DESIGNATED OR SHALL BE HAULED AWAY FROM THE SITE TO A COUNTY APPROVED DISPOSAL SITE. WHERE FILL MATERIAL IS TO BE PLACED ON NATURAL GROUND, IS SHALL BE STRIPPED OF ALL VEGETATION TO ACHIEVE A PROPER BOND WITH THE FILL MATERIAL. THE SURFACE OF THE GROUND SHALL BE SCARIFIED TO DEPTH OF 6" BEFORE FILL IS PLACED. WHERE NATURAL GROUND IS STEEPER THAN 5:1, IT SHALL BE BENCHMARKED AND THE FILL KEPT IN TO ACHIEVE STABILITY. WHERE NEW FILL AREAS ARE EXISTING, THE EXISTING FILL SHALL BE REMOVED UNTIL MATERIAL COMPACTED TO 90% RELATIVE COMPACTION IS EXPOSED. THEN THE NEW FILL MATERIAL SHALL BE PLACED IN UNIFORM LIFTS NOT EXCEEDING 6" IN UNCOMPACTED THICKNESS. BEFORE COMPACTION BEGINS, THE FILL SHALL BE BROUGHT TO A WATER CONTENT THAT WILL PERMIT PROPER COMPACTION BY EITHER 1) AERATING THE FILL IF IT IS TOO WET OR 2) MOISTENING THE FILL WITH WATER IF IT IS TOO DRY. EACH LIFT SHALL BE THOROUGHLY MIXED BEFORE COMPACTION TO ENSURE A UNIFORM DISTRIBUTION OF MOISTURE.
2. EXCESS CUT MATERIAL SHALL NOT BE SPREAD OR STOCKPILED ON THE SITE.
3. SURPLUS EARTH FILL MATERIAL SHALL BE PLACED IN A SINGLE (8" MAX) THICK LAYER COMPACTED TO WITHSTAND WEATHERING IN THE AREA(S) Delineated ON THE PLAN.
4. NO ORGANIC MATERIAL SHALL BE PLACED IN ANY FILL. NO TREES SHALL BE REMOVED OUTSIDE OF CUT, FILL OR ROADWAY AREAS.
5. THE UPPER 6" OF SUBGRADE BELOW DRIVEWAY ACCESS ROAD OR PARKING AREA SHALL BE COMPACTED TO 95% OF MAXIMUM DENSITY.
6. MAXIMUM CUT SLOPE SHALL BE 2 HORIZONTAL TO 1 VERTICAL. MAXIMUM FILL SLOPE SHALL BE 2 HORIZONTAL TO 1 VERTICAL.

LOCATION	CUT (C.Y.)	FILL (C.Y.)	NET (C.Y.)	MAX CUT HT.	MAX FILL HT.
RESIDENCE	94 CY	82 CY	12 (C)	1.5 FT	2.5 FT
ACCESSORY STRUCTURE	83 CY	0 CY	83 (C)	2.5 FT	0.0 FT
POOL/HARDSCAPE	351 CY	87 CY	264 (C)	6.0 FT	3.0 FT
HARDSCAPE	123 CY	5 CY	118 (C)	3.0 FT	0.5 FT
DRIVEWAY	8 CY	0 CY	8 (C)	0.5 FT	0.0 FT
OFF SITE IMPROVEMENTS	-	-	-	-	-
TOTAL	659 CY	174 CY	485 (C)	-	-

- NOTE: FILL VOLUMES INCLUDE 10% SHRINKAGE. EXCESS MATERIAL SHALL BE OFF HAULED TO A COUNTY APPROVED DUMP SITE.
7. NOTIFY SOILS ENGINEER TWO (2) DAYS PRIOR TO COMMENCEMENT OF ANY GRADING WORK TO COORDINATE THE WORK IN THE FIELD.
8. ALL MATERIALS FOR FILL SHOULD BE APPROVED BY THE SOILS ENGINEER BEFORE IT IS BROUGHT TO THE SITE.
9. THE UPPER 6" OF THE SUBGRADE SOIL SHALL BE SCARIFIED, MOISTURE CONDITIONED AND COMPACTED TO A MINIMUM RELATIVE COMPACTION OF 95%.
10. ALL AGGREGATE BASE MATERIAL SHALL BE COMPACTED TO A MINIMUM 95% RELATIVE COMPACTION.
11. BY AN PLAN REVIEW LETTER MUST BE REVIEWED AND APPROVED BY THE COUNTY GEOLOGIST PRIOR TO FINAL APPROVAL BY THE COUNTY ENGINEER FOR BUILDING OCCUPANCY.
12. THE PROJECT GEOTECHNICAL ENGINEER SHALL PERFORM COMPACTION TESTING AND PRESENT THE RESULTS TO THE COUNTY ENGINEERING INSPECTOR PRIOR TO THE CONSTRUCTION OF ANY PAVED AREA.
13. GRADING WORK BETWEEN OCTOBER 15TH AND APRIL 15TH IS AT THE DISCRETION OF THE SANTA CLARA COUNTY GRADING OFFICIAL.
14. TOTAL DISTURBED AREA FOR THE PROJECT _____ SF.
15. WDD NO. _____
16. THE INSPECTOR MAY VERIFY THAT A VALID NOTICE OF INTENT (NOI) HAS BEEN ISSUED BY THE STATE AND THAT A CURRENT AND TO DATE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IS AVAILABLE ON SITE.

TREE PROTECTION

1. FOR ALL TREES TO BE RETAINED WITH A CANOPY IN THE DEVELOPMENT AREA OR INTERFACES WITH THE LIMITS OF GRADING FOR ALL PROPOSED DEVELOPMENT ON SITE, THE TREES SHALL BE PROTECTED BY THE PLACEMENT OF RIGID TREE PROTECTIVE FENCING, CONSISTENT WITH THE COUNTY INTEGRATED LANDSCAPE GUIDELINES, AND MAXIMIZE THE TREE CANOPY.
- A. FENCING SHOULD BE PLACED ALONG THE OUTSIDE EDGE OF THE DRUPLINE OF THE TREE OR GROVE OF TREES.
- B. THE FENCING SHALL BE MAINTAINED THROUGHOUT THE SITE CONSTRUCTION PERIOD AND SHALL BE INSPECTED PERIODICALLY FOR DAMAGE AND PROPER FUNCTION.
- C. FENCING SHALL BE REPAIRED, AS NECESSARY, TO PROVIDE A PHYSICAL BARRIER FROM CONSTRUCTION ACTIVITIES.
- D. SIGNAGE STATING "WARNING- THIS FENCING SHALL NOT BE REMOVED WITHOUT PERMISSION FROM THE SANTA CLARA COUNTY PLANNING OFFICE (408) 299-5770. COUNTY OF SANTA CLARA TREE PROTECTION MEASURES MAY BE FOUND AT <http://www.sccplanning.gov> SHALL BE PLACED ON THE TREE PROTECTIVE FENCING UNTIL FINAL OCCUPANCY.
2. PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION ACTIVITY, TREE PROTECTIVE FENCING SHALL BE SECURELY IN PLACED AND INSPECTED BY THE LAND DEVELOPMENT ENGINEERING INSPECTOR.
3. SEE EXISTING TREE PROTECTION DETAILS FOR MORE INFORMATION.

ACCESS ROADS AND DRIVEWAYS

1. DRIVEWAY LOCATIONS SHALL BE AS SHOWN ON THE IMPROVEMENT PLANS WITH CENTERLINE STATIONING. THE MINIMUM CONCRETE THICKNESS SHALL BE 6 INCHES THROUGHOUT (WITH A MAXIMUM APPROACH SLOPE OF 1 1/4 INCHES PER FOOT).
2. ALL DRIVEWAY OR COMMON ACCESS ROAD SECTIONS IN EXCESS OF 15 LONGITUDINAL SLOPE MUST BE PAVED WITH A MINIMUM 2-INCH ASPHALT LIFT OR FULL DEPTH CONCRETE LIFT PRIOR TO ANY COMBUSTIBLE FRAMING.
3. THE OWNER AND PRIME CONTRACTOR ARE RESPONSIBLE FOR MAINTAINING PROJECT SITE ACCESS AND NEIGHBORHOOD ACCESS FOR EMERGENCY VEHICLES AND LOCAL RESIDENTS.
4. ROADWAYS DESIGNATED AS NOT COUNTY MAINTAINED ROADS AS SHOWN ON THE PLAN WILL NOT BE ELIGIBLE FOR COUNTY MAINTENANCE UNTIL THE ROADWAYS ARE IMPROVED (AT NO COST TO THE COUNTY) TO THE PUBLIC MAINTENANCE ROAD STANDARDS APPROVED BY THE BOARD OF SUPERVISORS AND IN EFFECT AT SUCH TIME THAT THE ROADWAYS ARE CONSIDERED FOR ACCEPTANCE INTO THE COUNTY'S ROAD SYSTEM.
5. ALL WORK IN THE COUNTY ROAD RIGHT-OF-WAY REQUIRES AN ENCROACHMENT PERMIT FROM THE ROADS AND AIRPORTS DEPARTMENT. EACH INDIVIDUAL ACTIVITY REQUIRES A SEPARATE PERMIT - I.E. CABLE, ELECTRICAL, GAS, SEWER, WATER, RETAINING WALLS, DRIVEWAY APPROACHES, FENCES, LANDSCAPING, TREE REMOVAL, STORM DRAINAGE IMPROVEMENTS, ETC..

STREET LIGHTING

1. PACIFIC GAS & ELECTRIC ELECTROLEUR SERVICE FEE SHALL BE PAID BY THE DEVELOPER AND/OR HIS AUTHORIZED REPRESENTATIVE.

SANITARY SEWER

1. THE SANITARY SEWER AND WATER UTILITIES SHOWN ON THESE PLANS ARE NOT PART OF THIS GRADING PERMIT AND ARE SHOWN FOR REFERENCE ONLY.
2. ALL MATERIALS AND METHODS OF CONSTRUCTION OF SANITARY SEWERS SHALL CONFORM TO THE SPECIFICATIONS OF THE JURISDICTION INVOLVED. INSPECTION OF SANITARY SEWER WORK SHALL BE DONE BY SAID JURISDICTION.

PORTLAND CEMENT CONCRETE

1. CONCRETE USED FOR STRUCTURAL PURPOSES SHALL BE CLASS "A" (6 SACK PER CUBIC YARD) AS SPECIFIED IN THE STATE STANDARD SPECIFICATIONS. CONCRETE PLACED MUST DEVELOP A MINIMUM STRENGTH FACTOR OF 2800 PSI IN A SEVEN-DAY PERIOD. THE CONCRETE MIX DESIGN SHALL BE UNDER THE CONTINUAL CONTROL OF THE COUNTY INSPECTOR.

AIR QUALITY, LANDSCAPING AND EROSION CONTROL

1. WATER ALL ACTIVE CONSTRUCTION AREAS AT LEAST TWICE DAILY.
2. COVER ALL TRUCKS HAULING SOIL, SAND, AND OTHER LOOSE MATERIALS OR REQUIRE ALL TRUCKS TO MAINTAIN AT LEAST TWO FEET OF FREEBOARD.
3. PAVE, APPLY WATER THREE TIMES DAILY, OR APPLY (NON-TOXIC) SOIL STABILIZERS ON ALL UNPAVED ACCESS ROADS, PARKING AREAS AND STAGING AREAS AT CONSTRUCTION SITES.
4. SWEEP DAILY (WITH WATER SWEEPERS) ALL PAVED ACCESS ROADS, PARKING AREAS AND STAGING AREAS AT CONSTRUCTION SITES. THE USE OF DRY POWDER SWEEPING IS PROHIBITED.
5. SWEEP STREETS DAILY (WITH WATER SWEEPERS) IF VISIBLE SOIL MATERIAL IS CARRIED ONTO ADJACENT PUBLIC STREETS. THE USE OF DRY POWDER SWEEPING IS PROHIBITED.
6. ALL CONSTRUCTION VEHICLES, EQUIPMENT AND DELIVERY TRUCKS SHALL HAVE A MAXIMUM IDLING TIME OF 5 MINUTES (AS REQUIRED BY THE CALIFORNIA AIRBORNE TOXIC CONTROL MEASURE TITLE 13, SECTION 2485 OF CALIFORNIA CODE OF REGULATIONS (CCR)). ENGINES SHALL BE SHUT OFF IF CONSTRUCTION REQUIRES LONGER IDLING TIME UNLESS NECESSARY FOR PROPER OPERATION OF THE VEHICLE.
7. ALL VEHICLE SPEEDS ON UNPAVED ROADS SHALL BE LIMITED TO 15 MILES PER HOUR.
8. ALL CONSTRUCTION EQUIPMENT SHALL BE MAINTAINED AND PROPERLY TUNED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS. ALL EQUIPMENT SHALL BE CHECKED BY A CERTIFIED MECHANIC AND DETERMINED TO BE RUNNING IN PROPER CONDITION PRIOR TO OPERATION.
9. POST A SIGN THAT IS AT LEAST 32 SQUARE FEET MINIMUM 2 INCHES LETTER HEIGHT VISIBLE NEAR THE ENTRANCE OF CONSTRUCTION SITE THAT IDENTIFIES THE FOLLOWING REQUIREMENTS. OBTAIN ENCROACHMENT PERMIT FOR SIGN FROM ROADS DEPARTMENT OR OTHER APPLICABLE AGENCY IF REQUIRED.
- A. 15 MINUTES MAXIMUM IDLING TIME OF VEHICLES
- B. 5 MINUTES MAXIMUM IDLING TIME OF VEHICLES
- C. TELEPHONE NUMBER TO CONTACT THE BAY AREA AIR QUALITY MANAGEMENT DISTRICT REGARDING DUST COMPLAINTS. NOTE PHONE NUMBER: BAY AREA AIR QUALITY MANAGEMENT DISTRICT AIR POLLUTION COMPLAINT HOTLINE OF 1-800-334-6367.
10. ALL FILL SLOPES SHALL BE COMPACTED AND LEFT IN A SMOOTH AND FIRM CONDITION CAPABLE OF WITHSTANDING WEATHERING.
11. ALL EXPOSED DISTURBED AREAS SHALL BE SEDED WITH BROME SEED SPREAD AT THE RATE OF 5 LB. PER 1000 SQUARE FEET (OR APPROVED EQUIV.) SEEDING AND WATERING SHALL BE MAINTAINED AS REQUIRED TO ENSURE GROWTH.
12. ALL DITCHES SHALL BE LINED PER COUNTY STANDARD SDR.
13. ALL STORM DRAINAGE STRUCTURES SHALL BE INSTALLED WITH EFFECTIVE ENTRANCE & OUTFALL EROSION CONTROLS E.G. SACKED CONCRETE RIP-RAP. ENERGY DISSIPATORS SHALL BE INSTALLED AT ALL DITCH OUTFALLS. WHERE OUTFALLS ARE NOT INTO AN EXISTING CREEK OR WATER COURSE, RUNOFF SHALL BE RELEASED TO SHEET FLOW.
14. PRIOR TO GRADING COMPLETION AND RELEASE OF THE BOND, ALL GRADED AREAS SHALL BE RESEDED IN CONFORMANCE WITH THE COUNTY GRADING ORDINANCE TO MINIMIZE THE VISUAL IMPACTS OF THE GRADE SLOPES AND REDUCE THE POTENTIAL FOR EROSION OF THE SUBJECT SITE.
15. PERMANENT LANDSCAPING SHOWN ON THE ATTACHED LANDSCAPE PLAN MUST BE INSTALLED AND FIELD APPROVED BY THE COUNTY PLANNING OFFICE PRIOR TO FINAL APPROVAL BY THE COUNTY ENGINEER, AND FINAL OCCUPANCY RELEASE BY THE BUILDING INSPECTION OFFICE.
16. THE OWNER SHALL PREPARE AND PRESENT A WINTERIZATION REPORT TO THE COUNTY INSPECTOR FOR REVIEW PRIOR TO OCTOBER 15TH OF EVERY YEAR.
17. THE OWNER, CONTRACTOR, AND ANY PERSON PERFORMING CONSTRUCTION ACTIVITIES SHALL INSTALL AND MAINTAIN CONSTRUCTION BEST MANAGEMENT PRACTICES (BMPs) ON THE PROJECT SITE AND WITHIN THE SANTA CLARA COUNTY ROAD RIGHT-OF-WAY THROUGHOUT THE DURATION OF THE CONSTRUCTION AND UNTIL THE ESTABLISHMENT OF PERMANENT STABILIZATION AND SEDIMENT CONTROL TO PREVENT THE DISCHARGE OF POLLUTANTS INCLUDING SEDIMENT, CONSTRUCTION MATERIALS, EXCAVATED MATERIALS, AND WASTE INTO THE SANTA CLARA COUNTY RIGHT-OF-WAY, STORM SEWER WATERWAYS, ROADWAY INFRASTRUCTURE. BMPs SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING:
- A. PREVENTION OF POLLUTANTS IN STORM WATER DISCHARGES FROM THE CONSTRUCTION SITE AND THE CONTRACTOR'S MATERIAL AND EQUIPMENT LAYDOWN / STAGING AREAS.
- B. PREVENTION OF TRACKING OF MUD, DIRT, AND CONSTRUCTION MATERIALS ONTO THE PUBLIC ROAD RIGHT-OF-WAY.
- C. PREVENTION OF DISCHARGE OF WATER RUN-OFF DURING DRY AND WET WEATHER SHOWN ON THE PLANS OR BY THE OWNER OR HIS SUCCESSOR PROPERTY OWNERS ARE RESPONSIBLE FOR THE ADEQUACY AND CONTINUED MAINTENANCE OF THESE FACILITIES IN A MANNER WHICH WILL PRECLUDE ANY HAZARD TO LIFE, HEALTH, OR DAMAGE TO ADJOINING PROPERTY, CONSISTENT WITH NPDES PERMIT CAS612008 / ORDER NO. R2-2009-0047 AND NPDES PERMIT CAS000004 / ORDER NO. 2013-0001-DWQ.
18. THE OWNER, CONTRACTOR, AND ANY PERSON PERFORMING CONSTRUCTION ACTIVITIES SHALL ENSURE THAT ALL TEMPORARY CONSTRUCTION FACILITIES, INCLUDING BUT NOT LIMITED TO CONSTRUCTION MATERIALS, DELIVERIES, HAZARDOUS AND NON-HAZARDOUS MATERIAL STORAGE, EQUIPMENT, TOOLS, PORTABLE TOILETS, CONCRETE WASHOUT, GARBAGE CONTAINERS, LAYDOWN YARDS, SECONDARY CONTAINMENT AREAS, ETC. ARE LOCATED OUTSIDE THE SANTA CLARA COUNTY ROAD RIGHT-OF-WAY.
19. EROSION CONTROL PLAN IS A GUIDE AND SHALL BE AMENDED AS NECESSARY TO PREVENT EROSION AND ILLIOT DISCHARGES ON A YEAR AROUND BASIS, DEPENDING ON THE SEASON, WEATHER, AND FIELD CONDITIONS. EROSION CONTROL MEASURES IN ADDITION TO THOSE NOTED IN THE PERMITTED PLANS MAY BE NECESSARY TO PREVENT FAILURE TO INSTALL SITE SITE AND SITUATIONALLY APPROPRIATE EROSION CONTROL MEASURES MAY RESULT IN VIOLATIONS, FINES, AND A STOPPAGE OF WORK.

STORM DRAINAGE AND STORMWATER MANAGEMENT

1. DEVELOPER IS RESPONSIBLE FOR ALL NECESSARY DRAINAGE FACILITIES WHETHER SHOWN ON THE PLANS OR BY THE OWNER OR HIS SUCCESSOR PROPERTY OWNERS ARE RESPONSIBLE FOR THE ADEQUACY AND CONTINUED MAINTENANCE OF THESE FACILITIES IN A MANNER WHICH WILL PRECLUDE ANY HAZARD TO LIFE, HEALTH, OR DAMAGE TO ADJOINING PROPERTY, CONSISTENT WITH NPDES PERMIT CAS612008 / ORDER NO. R2-2009-0047 AND NPDES PERMIT CAS000004 / ORDER NO. 2013-0001-DWQ.
2. DROP INLETS SHALL BE COUNTY STANDARD TYPE 5 UNLESS OTHERWISE NOTED ON THE PLANS. THE DEVELOPER'S ENGINEER SHALL BE RESPONSIBLE FOR THE PROPER LOCATION OF DROP INLETS. WHERE STREET PROFILE GRADE EXCEEDS 6% DROP INLETS SHALL BE SET AT 500 ANGLE CURB LINE TO ACCEPT WATER OR AS SHOWN ON THE PLANS.
3. WHERE CULVERTS ARE INSTALLED THE DEVELOPER SHALL BE RESPONSIBLE FOR GRADING THE OUTLET DITCH TO DRAIN TO AN EXISTING SWALE OR TO AN OPEN AREA FOR SHEET FLOW.
4. UPON INSTALLATION OF DRIVEWAY CONNECTIONS, PROPERTY OWNERS SHALL PROVIDE FOR THE UNINTERRUPTED FLOW OF WATER IN ROADSIDE DITCHES.
5. THE COUNTY SHALL INSPECT UNDERGROUND DRAINAGE IMPROVEMENTS AND STORMWATER MANAGEMENT FEATURES PRIOR TO BACKFILL.

AS-BUILT PLANS STATEMENT

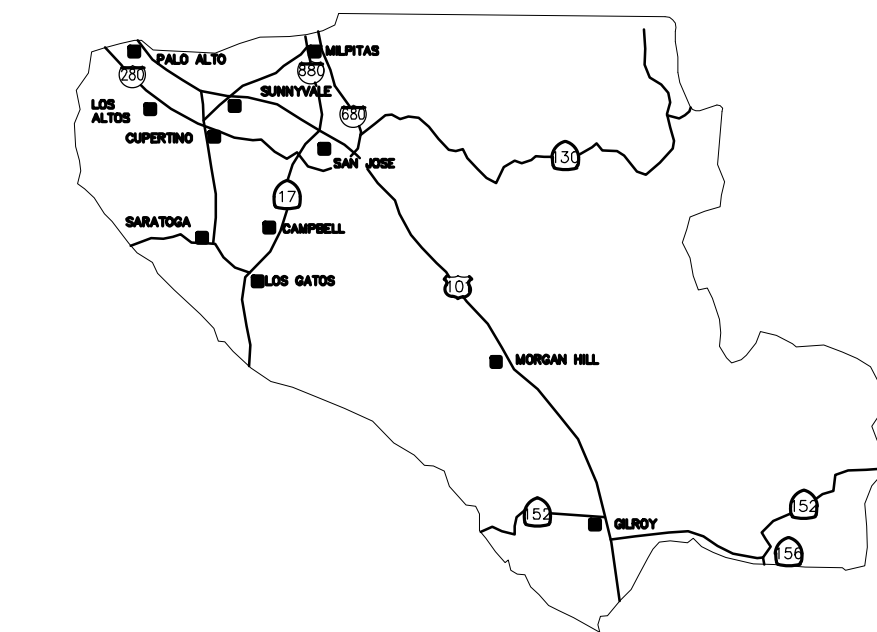
THIS IS A TRUE COPY OF THE AS-BUILT PLANS. THERE () WERE () WERE NOT) MINOR FIELD CHANGES - MARKED WITH THE SYMBOL ("). THERE () WERE () WERE NOT) PLAN REVISIONS INDICATING SIGNIFICANT CHANGES REVIEWED BY THE COUNTY ENGINEER AND MARKED WITH THE SYMBOL Δ.

DATE _____ SIGNATURE _____

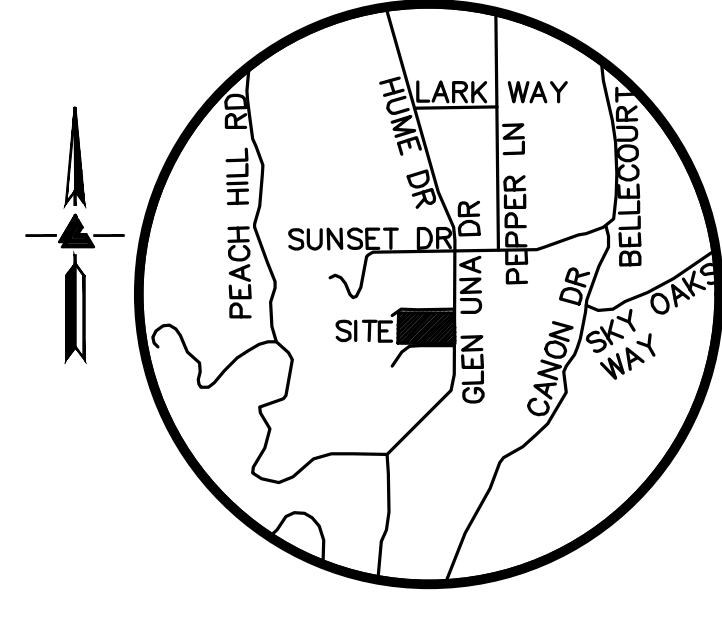
NOTE: THIS STATEMENT IS TO BE SIGNED BY THE PERSON AUTHORIZED BY THE COUNTY ENGINEER TO PERFORM THE INSPECTION WORK. A REPRODUCIBLE COPY OF THE AS-BUILT PLANS MUST BE FURNISHED TO THE COUNTY ENGINEER AFTER CONSTRUCTION.

GEOTECHNICAL ENGINEER OBSERVATION

1. A CONSTRUCTION OBSERVATION LETTER FROM THE RESPONSIBLE GEOTECHNICAL ENGINEER AND ENGINEERING GEOLOGIST DETAILING CONSTRUCTION OBSERVATIONS AND CERTIFYING THAT THE WORK WAS DONE IN ACCORDANCE WITH THE RECOMMENDATIONS IN THE GEOTECHNICAL AND GEOLOGIC REPORTS SHALL BE SUBMITTED PRIOR TO THE GRADING COMPLETION AND RELEASE OF THE BOND.



COUNTY LOCATION MAP



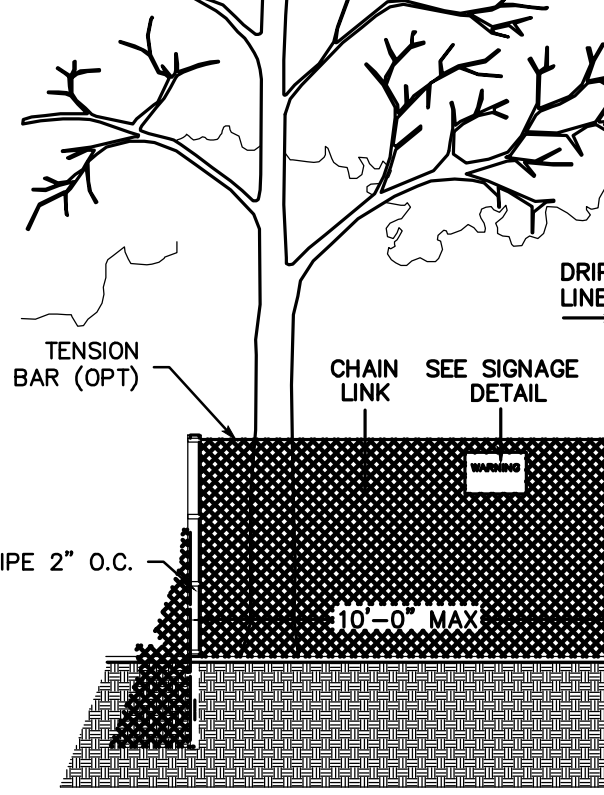
VICINITY MAP
NO SCALE

PROJECT TITLE
VUPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS
UNINCORPORATED

SEE SHEET C-1.1 FOR LEGEND & ABBREVIATIONS

SCOPE OF WORK

1. THE DEVELOPER IS RESPONSIBLE FOR THE INSTALLATION OF THE WORK PROPOSED ON THE EROSION CONTROL PLAN. THE ENGINEER OF RECORD IS RESPONSIBLE FOR THE DESIGN OF THE EROSION CONTROL PLANS AND ANY MODIFICATIONS OF THE EROSION CONTROL PLANS TO PREVENT ILLIOT DISCHARGES FROM THE SITE DURING CONSTRUCTION.
2. CONSTRUCTION OF NEW HOUSE AND ADU
3. CONSTRUCTION OF DRIVEWAY AND PARKING AREAS
4. CONSTRUCTION OF PATIO AND PATHWAY
5. CONSTRUCTION OF POOL
6. CONSTRUCTION OF STORM WATER FACILITIES
7. CONSTRUCTION RETAINING WALLS



EXISTING TREE PROTECTION DETAILS

1. PRIOR TO THE COMMENCEMENT OF ANY GRADING, TREE PROTECTIVE FENCING SHALL BE IN PLACE IN ACCORDANCE WITH THE TREE PRESERVATION PLAN AND INSPECTED BY A CERTIFIED ARBORIST. THE ARBORIST SHALL MONITOR CONSTRUCTION ACTIVITY TO ENSURE THAT THE TREE PROTECTION MEASURES ARE IMPLEMENTED AND ADHERED TO DURING CONSTRUCTION. THIS CONDITION SHALL BE INCORPORATED INTO THE GRADING PLANS.
2. FENCE SHALL BE MINIMUM 5 FEET TALL, CONSTRUCTED OF STURDY MATERIAL (CHAIN-LINK OR EQUIVALENT STRENGTH/ DURABILITY).
3. FENCE SHALL BE SUPPORTED BY VERTICAL POSTS DRIVEN 2 FEET (MIN) INTO THE GROUND AND SPACED NOT MORE THAN 10 FEET APART.
4. TREE FENCING SHALL BE MAINTAINED THROUGHOUT THE SITE DURING THE CONSTRUCTION PERIOD AND SHALL BE INSPECTED PERIODICALLY FOR DAMAGE AND PROPER FUNCTION, REPAIRED AS NECESSARY TO PROVIDE A PHYSICAL BARRIER FROM CONSTRUCTION ACTIVITIES, AND REMAIN IN PLACE UNTIL THE FINAL INSPECTION.
5. A SIGN THAT INCLUDES THE WORDS, "WARNING: THIS FENCE SHALL NOT BE REMOVED WITHOUT THE EXPRESSED PERMISSION OF THE SANTA CLARA COUNTY PLANNING OFFICE," SHALL BE SECURELY ATTACHED TO THE FENCE IN A VISUALLY PROMINENT LOCATION.

SHEET INDEX

C-1.0	TITLE SHEET
C-1.1	OVERALL SITE PLAN
C-2.0	GRADING & DRAINAGE PLAN
C-3.0	UTILITY PLAN
C-4.0	SITE SECTIONS
ER-1	EROSION CONTROL PLAN
BMP-1	BEST MANAGEMENT PRACTICES
BMP-2	BEST MANAGEMENT PRACTICES
HYD-1	IMPERVIOUS SURFACE EXHIBIT
HYD-2	PROPOSED DRAINAGE EXHIBIT
	TOPOGRAPHIC MAP

LEA & BRAZE ENGINEERING, INC.
CIVIL ENGINEERS • LAND SURVEYORS
BAY AREA REGION: 2495 INDUSTRIAL PKWY WEST, HAYWARD, CALIFORNIA 94545
SACRAMENTO REGION: 3017 DOUGLAS BLVD., # 300, ROSEVILLE, CA 95661
(P) (510) 887-4086 (P) (916)966-1338
(F) (510) 887-3019 (F) (916)797-7363
WWW.LEABRAZE.COM

DATE: 07/21/22
LB#: 2221289

Revision	Date	APN	Sheet
Revision 1	Date	510-26-076	01
Revision 2	Date	Co. File	of
Revision 3	Date		11

APPLICANT: SINGH

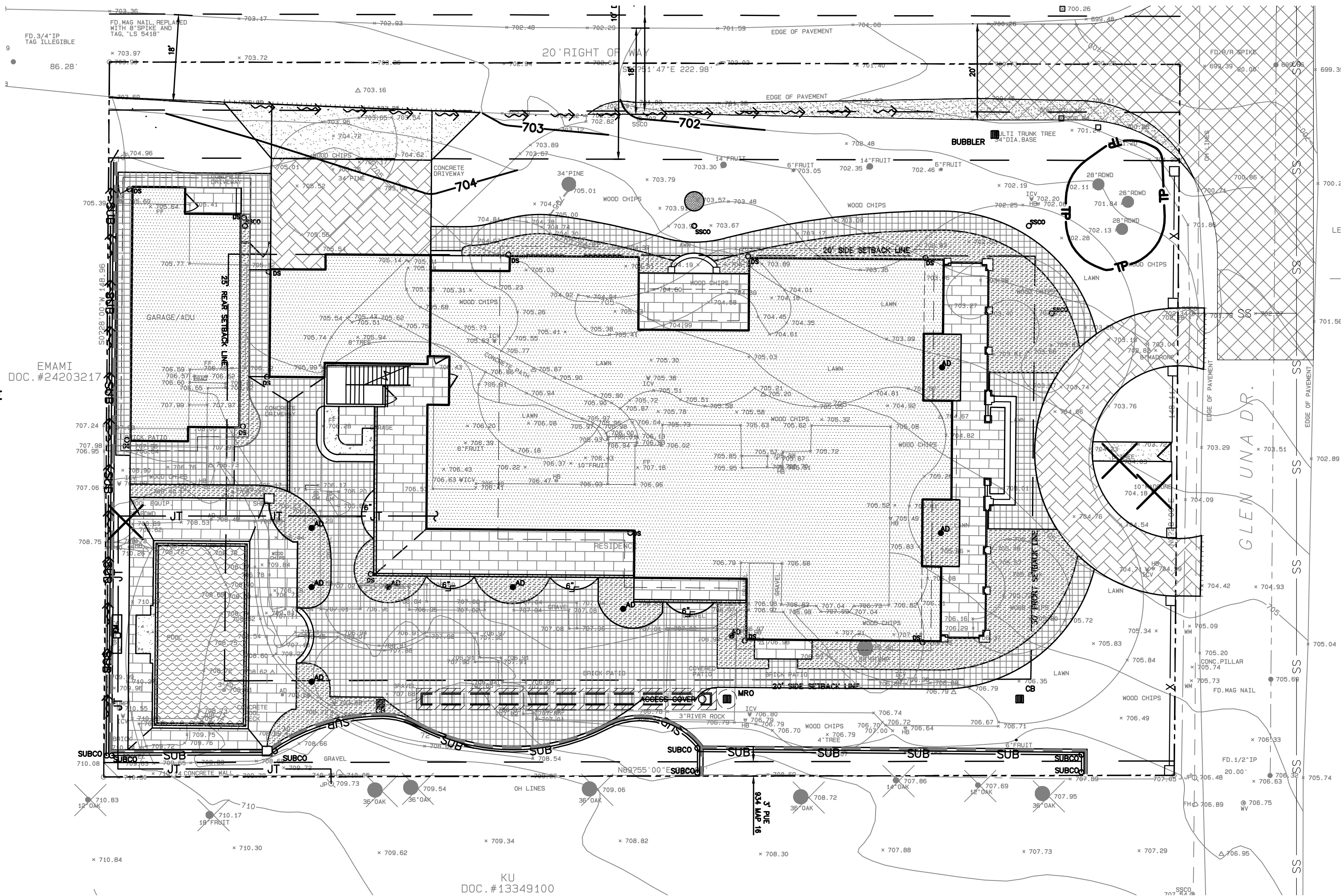
ROAD: GLEN UNA DRIVE COUNTY FILE NO.:

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	BOUNDARY
---	---	PROPERTY LINE
---	---	RETAINING WALL
---	---	LANDSCAPE RETAINING WALL
---	---	RAINWATER TIGHTLINE
---	---	SUBDRAIN LINE
---	---	TIGHTLINE
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	WATER LINE
---	---	GAS LINE
---	---	STORM DRAIN PRESSURE LINE
---	---	SANITARY SEWER PRESSURE LINE
---	---	JOINT TRENCH
---	---	SET BACK LINE
---	---	CONCRETE VALLEY GUTTER
---	---	EARTHEN SWALE
---	---	CATCH BASIN
---	---	JUNCTION BOX
---	---	AREA DRAIN
---	---	CURB INLET
---	---	STORM DRAIN MANHOLE
---	---	FIRE HYDRANT
---	---	SANITARY SEWER MANHOLE
---	---	STREET SIGN
---	---	SPOT ELEVATION
---	---	FLOW DIRECTION
---	---	DEMOLISH/REMOVE
---	---	BENCHMARK
---	---	CONTOURS
---	---	TREE TO BE REMOVED
---	---	TREE PROTECTION FENCING

ABBREVIATIONS

AB	AGGREGATE BASE	LF	LINEAR FEET
AC	ASPHALT CONCRETE	MAX	MAXIMUM
ACC	ACCESSIBLE	MH	MANHOLE
AD	AREA DRAIN	MIN	MINIMUM
BC	BEGINNING OF CURVE	MON.	MONUMENT
B & D	BEARING & DISTANCE	MRO	METERED RELEASE OUTLET
BM	BENCHMARK	(N)	NEW
BUB	BUBBLER BOX	NO.	NUMBER
BW/FG	BOTTOM OF WALL/FINISH GRADE	NTS	NOT TO SCALE
CB	CATCH BASIN	O.C.	ON CENTER
C & G	CURB AND GUTTER	O/	OVER
C	CENTER LINE	(PA)	PLANTING AREA
CPP	CORRUGATED PLASTIC PIPE (SMOOTH INTERIOR)	PED	PEDESTRIAN
CO	CLEANOUT	PIV	POST INDICATOR VALVE
COTG	CLEANOUT TO GRADE	PSS	PUBLIC SERVICES EASEMENT
CONC	CONCRETE	P	PROPERTY LINE
CONST	CONSTRUCT or -TION	PP	POWER POLE
CONC COR	CONCRETE CORNER	PUE	PUBLIC UTILITY EASEMENT
CY	CUBIC YARD	PVC	POLYVINYL CHLORIDE
D	DIAMETER	R	RADIUS
DI	DROP INLET	RCP	REINFORCED CONCRETE PIPE
DIP	DUCTILE IRON PIPE	RIM	RIM ELEVATION
EA	EACH	RW	RAINWATER
EC	END OF CURVE	R/W	RIGHT OF WAY
EG	EXISTING GRADE	S	SLOPE
EL	ELEVATIONS	S.A.D.	SEE ARCHITECTURAL DRAWINGS
EP	EDGE OF PAVEMENT	SAN	SANITARY
EQ	EQUIPMENT	SD	STORM DRAIN
EW	EACH WAY	SDMH	STORM DRAIN MANHOLE
(E)	EXISTING	SHT	SHEET
FC	FACE OF CURB	S.L.D.	SEE LANDSCAPE DRAWINGS
FF	FINISHED FLOOR	SPEC	SPECIFICATION
FG	FINISHED GRADE	SS	SANITARY SEWER
FH	FIRE HYDRANT	SSCO	SANITARY SEWER CLEANOUT
FL	FLOW LINE	SSMH	SANITARY SEWER MANHOLE
FS	FINISHED SURFACE	ST.	STREET
G	GAGE OR GAUGE	STA	STATION
GB	GRADE BREAK	STD	STANDARD
HDPE	HIGH DENSITY CORRUGATED POLYETHYLENE PIPE	STRUC	STRUCTURAL
HORIZ	HORIZONTAL	T	TELEPHONE
HI PT	HIGH POINT	TC	TOP OF CURB
H&T	HUB & TACK	TOW	TOP OF WALL
ID	INSIDE DIAMETER	TEMP	TEMPORARY
INV	INVERT ELEVATION	TP	TOP OF PAVEMENT
JB	JUNCTION BOX	TW/FG	TOP OF WALL/FINISH GRADE
JT	JOINT TRENCH	TYP	TYPICAL
JP	JOINT UTILITY POLE	VC	VERTICAL CURVE
L	LENGTH	VCP	VITRIFIED CLAY PIPE
LNDG	LANDING	VERT	VERTICAL
		W/	WITH
		W/ WL	WATER LINE
		WM	WATER METER
		WWF	WELDED WIRE FABRIC



NOTES

ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS.

BUILDING FOOTPRINTS ARE SHOWN TO FINISHED MATERIAL (STUCCO/SIDING) AT GROUND LEVEL.

FINISH FLOOR ELEVATIONS ARE TAKEN AT DOOR THRESHOLD (EXTERIOR).

THE AREA OF THE SURVEYED LOT IS 33,120± SQUARE FEET / 0.76± ACRES

EASEMENT NOTE

EASEMENTS ARE SHOWN PER PRELIMINARY TITLE REPORT ISSUED BY FIDELITY NATIONAL TITLE COMPANY, ORDER NO. FSBC-2062004769-DT, DATED NOVEMBER 24, 2020.

RETAINING WALL NOTES

- TW/FG REPRESENTS FINISHED EARTHEN GRADE OR PAVEMENT ELEVATION AT TOP OF WALL, NOT ACTUAL TOP OF WALL MATERIAL. BW/FG REPRESENTS FINISH EARTHEN GRADE OR PAVEMENT ELEVATION AT BOTTOM OF WALL NOT INCLUDING FILL FOUNDATION GRADES INDICATED ON THESE PLANS REFER TO THE FINISHED GRADES ADJACENT TO THE RETAINING WALL, NOT INCLUDING FOOTING, FREEBOARD, ETC.
- DIMENSIONS SHOWN IN BRACKETS SHOWN AS [X.X"] DENOTE THE EFFECTIVE WALL HEIGHT ONLY. THE ACTUAL WALL HEIGHT AND DEPTH MAY DIFFER DUE TO CONSTRUCTION REQUIREMENTS.
- REFER TO SPECIFIC WALL CONSTRUCTION DETAIL FOR STRUCTURAL ELEMENTS, FREEBOARD, AND EMBEDMENT.
- REFER TO ARCHITECTURAL, LANDSCAPE ARCHITECTURE, AND/OR STRUCTURAL PLANS FOR DETAILS, WALL ELEVATIONS, SUBDRAINAGE, WATERPROOFING, FINISHES, COLORS, STEEL REINFORCING, MATERIALS, ETC. PROVIDE CLIPS OR OTHER MEANS OF SECURING FINISH MATERIALS AS NECESSARY (WET SET INTO THE WALL).
- ALL RETAINING WALLS SHOULD HAVE A BACK-OF-WALL SUB-SURFACE DRAINAGE SYSTEM INCLUDING WEEPHOLES TO PREVENT HYDROSTATIC PRESSURE.
- SEE DETAIL SHEET FOR SPECIFIC INFORMATION.
- PROVIDE GUARDRAIL (WHERE APPLICABLE AND DESIGNED BY OTHERS) AS REQUIRED FOR GRADE SEPARATION OF 30 INCHES OR MORE MEASURED 5' HORIZONTALLY FROM FACE OF WALL, PER CBC.

BENCHMARK

CITY OF LOS GATOS BENCHMARK "RM27" "C" IN THE WORD CASTING ON C.B. WEST SIDE OF QUITO ROAD SOUTH OF MONTEWOOD DRIVE. ELEVATION = 444.22' (NAVD 88 DATUM)

SITE BENCHMARK

ELEVATIONS SHOWN ON THIS MAP ARE BASED ON A HIGH PRECISION GPS READING ON AN INITIAL CONTROL POINT AS ESTABLISHED WITH LEICHS HxGN SMARTNET (NAVD 88 DATUM)

TREE NOTE

TREE SIZE, TYPE AND DIAPHRANES ARE BASED ON A VISUAL OBSERVATION. FINAL DETERMINATION SHOULD BE MADE BY THE PROJECT ARBORIST.

BASIS OF BEARING

THE BEARINGS SHOWN ON THIS MAP ARE BASED ON THE CENTER LINE OF GLEN UNA DRIVE AS FOUND MONUMENTED AND RECORDED AS NORTH 00°28'00" EAST ON THE RECORD OF SURVEY FILED IN BOOK 403 OF MAPS, PAGE 26, SANTA CLARA COUNTY RECORDS.

FEMA FLOOD NOTE

FLOOD ZONE: D

100-YEAR BASE FLOOD ELEVATION (BFE) NOT DEFINED PER CURRENT FLOOD INSURANCE RATE MAP (FIRM)

FEMA FLOOD INSURANCE RATE MAP NO.: 06087C0110E EFFECTIVE DATE: MAY 16, 2012

UTILITY NOTE

ALL UNDERGROUND PIPE TYPES, SIZES AND LOCATION SHOWN ON THIS SURVEY ARE BASED ON VISUAL OBSERVATION. ANY USE OF THIS INFORMATION SHOULD BE VERIFIED, BEFORE ITS USE, WITH THE CONTROLLING MUNICIPALITY OR UTILITY PROVIDER. THIS SURVEY MAKES NO GUARANTEE OF THE INSTALLED ACTUAL LOCATION, DEPTHS OR SIZE.

OWNER'S INFORMATION

OWNER: KALYANI & MURTHY VAPPALA
15581 GLEN UNA DRIVE
LOS GATOS, CA

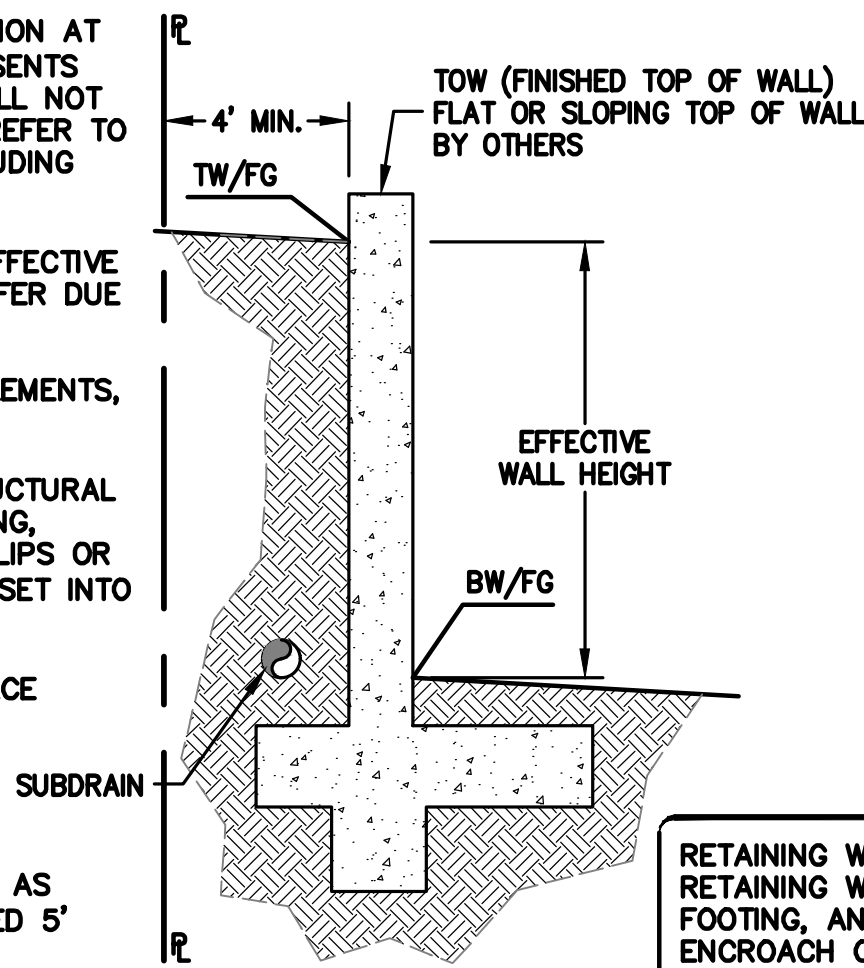
APN: 510-26-007

REFERENCES

THIS GRADING AND DRAINAGE PLAN IS SUPPLEMENTAL TO:
1. TOPOGRAPHIC SURVEY BY ROBERT J. CRAIG, ENTITLED: "TOPOGRAPHIC MAP" 15581 GLEN UNA DRIVE LOS GATOS, CA DATED: 1-23-2020 JOB# C-19143

2. SITE PLAN BY AADIARCH ENTITLED: "NEW RESIDENCE" 15581 GLEN UNA DRIVE LOS GATOS, CA

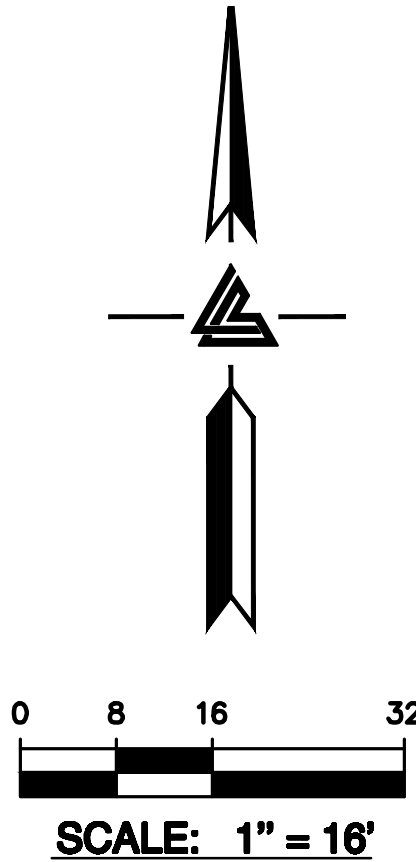
THE CONTRACTOR SHALL REFER TO THE ABOVE NOTED SURVEY AND PLAN, AND SHALL VERIFY BOTH EXISTING AND PROPOSED ITEMS ACCORDING TO THEM.



* BUILDING PAD NOTE:
ADJUST PAD LEVEL AS REQUIRED. REFER TO STRUCTURAL PLANS FOR SLAB SECTION OR CRAWL SPACE DEPTH TO ESTABLISH PAD LEVEL.

NOTE:
FOR CONSTRUCTION STAKING SCHEDULING OR QUOTATIONS PLEASE CONTACT ALEX ABAYA AT LEA & BRAZE ENGINEERING (510)887-4086 EXT 116. aabaya@leabraz.com

RETAINING WALL NOTE: PROPOSED RETAINING WALLS, INCLUDING THE FOOTING, AND THE SUBDRAIN SHALL NOT ENCRUCH ON NEIGHBORING PROPERTY.

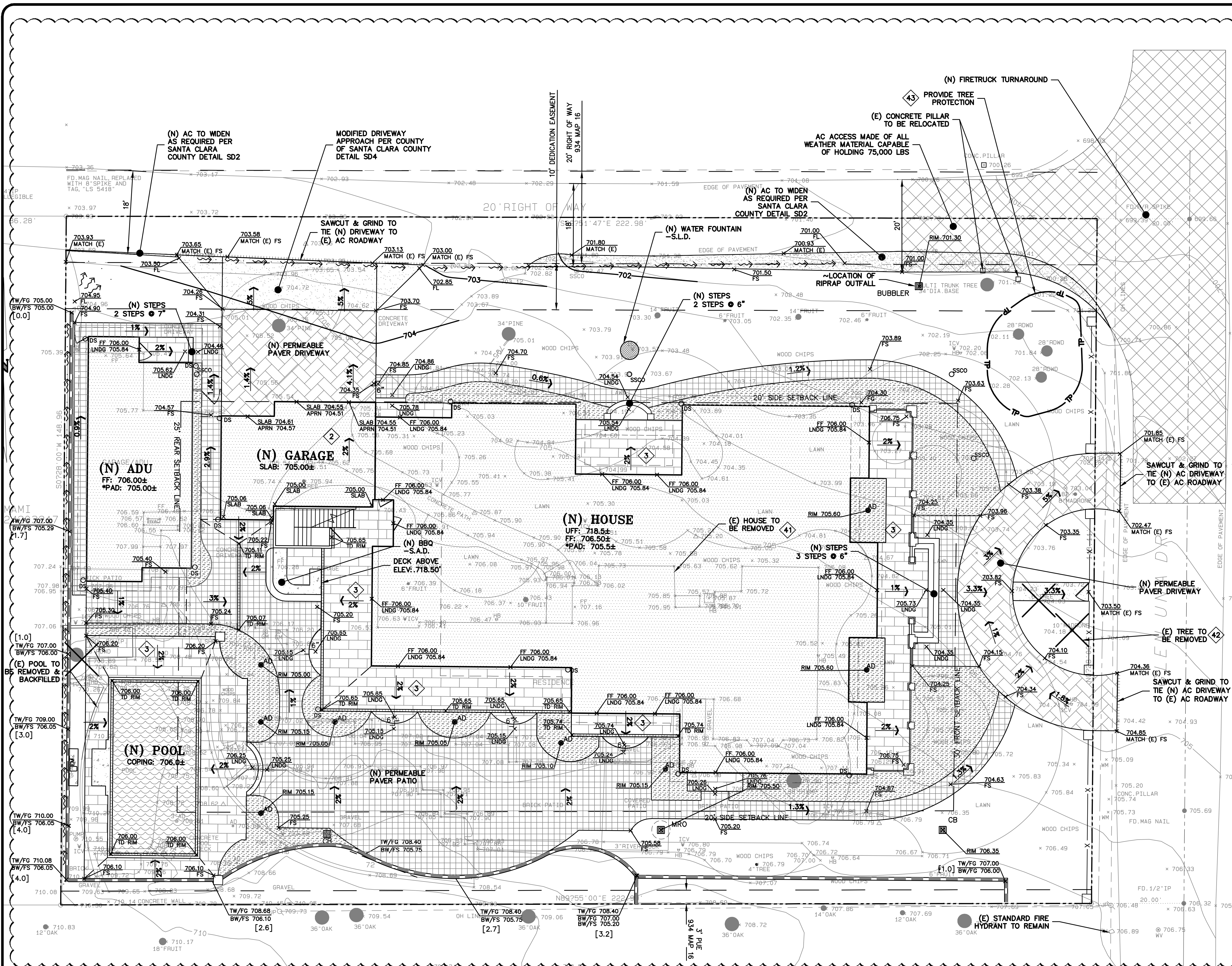


LEA & BRAZE ENGINEERING, INC.
CIVIL ENGINEERS & LAND SURVEYORS
REGIONAL OFFICES:
MAIN OFFICE: 15581 GLEN UNA DRIVE
LOS GATOS, CA 95030
(510) 887-4086
SAN JOSE
WWW.LEABRAZE.COM

VAPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA
SANTA CLARA COUNTY
APN: 510-26-007

OVERALL SITE PLAN

PLAN CHECK	MR
11-16-22	
REVISIONS	BY
JOB NO:	2221289
DATE:	07-21-22
SCALE:	1" = 20'
DESIGN BY:	MR
CHECKED BY:	DY
SHEET NO:	



FLATWORK KEYNOTES 1 TO 8

1 FINISHED GRADES AT BUILDING PERIMETER SHALL BE SLOPED AT A MINIMUM OF 5% FOR THE FIRST 10' AWAY FROM THE BUILDING PER CBC 1804.4 OR TO AN APPROVED DRAINAGE SWALE OR STRUCTURE. GRADES SHALL CONTINUE TO SLOPE TOWARDS POSITIVE DRAINAGE AND A POSITIVE OUTFALL. MAINTAIN 8" CLEARANCE BETWEEN FINISH EARTHEN GRADE AND BOTTOM OF MUD SILL AT ALL TIMES PER CBC 2304.12.1.2 UNLESS STRUCTURAL DETAILING ALLOWS LESS. REFER TO STRUCTURAL PLANS FOR FOUNDATION DESIGN AND DETAILS.

2 SLOPE GARAGE SLAB 1% MINIMUM (1/8" PER FOOT) FROM BACK TO FRONT TO ALLOW FOR ADEQUATE DRAINAGE. MAINTAIN 1/2" TO 1" LIP BETWEEN GARAGE SLAB AND DRIVEWAY. SEE PLANS FOR SPECIFIC DROP

3 PROVIDE 2% SLOPE ACROSS FLAT WORK AND/OR PAVING PER CBC 1804.4. SLOPE TOWARDS POSITIVE DRAINAGE AS SHOWN ON PLAN.

4 (N) AC DRIVEWAY. SEE DETAIL X ON SHEET C-X.

5 GRIND AC TO TIE (N) AC INTO (E) AC PAVING. SEE DETAIL X ON SHEET C-X.

6 INSTALL (N) CONCRETE PAVER DRIVEWAY. SEE DETAIL X ON SHEET C-X.

7 (N) CONCRETE PATIOS/WALKWAYS. SEE DETAIL X ON SHEET C-X.

8 (N) CONCRETE PATIO IN POOL AREA. SEE DETAIL X ON SHEET C-X.

DEMOLITION KEYNOTES 41 TO 43

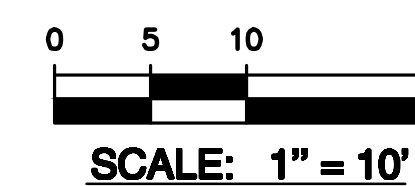
41 DEMOLISH (E) IMPROVEMENTS AS NECESSARY TO ACCOMMODATE (N) CONSTRUCTION. NO DEMOLITION SHALL COMMENCE WITHOUT REQUIRED DEMOLITION PERMITS.

42 REMOVE (E) TREE. CONTRACTOR SHALL OBTAIN THE PROPER TREE REMOVAL PERMITS AS REQUIRED.

43 PROVIDE TREE PROTECTION AROUND TREES TO REMAIN. SEE DETAIL 6 ON SHEET ER-2.

HATCH LEGEND

- PERVIOUS PAVER DRIVEWAY
- FIRST FLOOR
- PERVIOUS PAVER PATIOS
- STONE PATIO



NOTE:
FOR CONSTRUCTION STAKING SCHEDULING OR QUOTATIONS PLEASE CONTACT ALEX ABAYA AT LEA & BRAZE ENGINEERING (510)887-4086 EXT 116. aabaya@leabrazeng.com

*** BUILDING PAD NOTE:**
ADJUST PAD LEVEL AS REQUIRED. REFER TO STRUCTURAL PLANS FOR SLAB SECTION OR CRAWL SPACE DEPTH TO ESTABLISH PAD LEVEL.

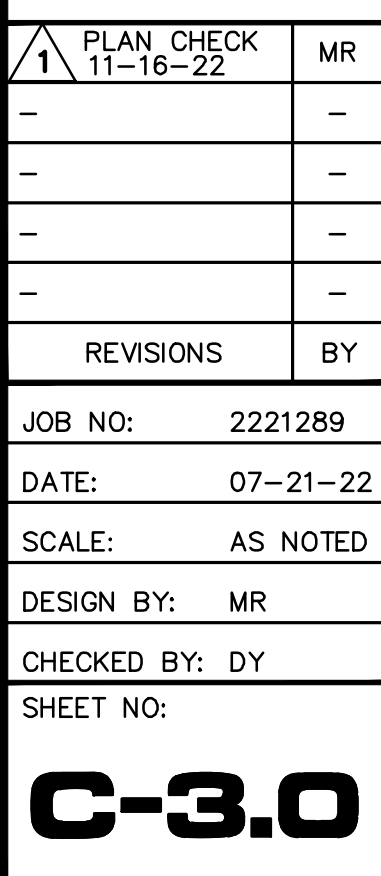
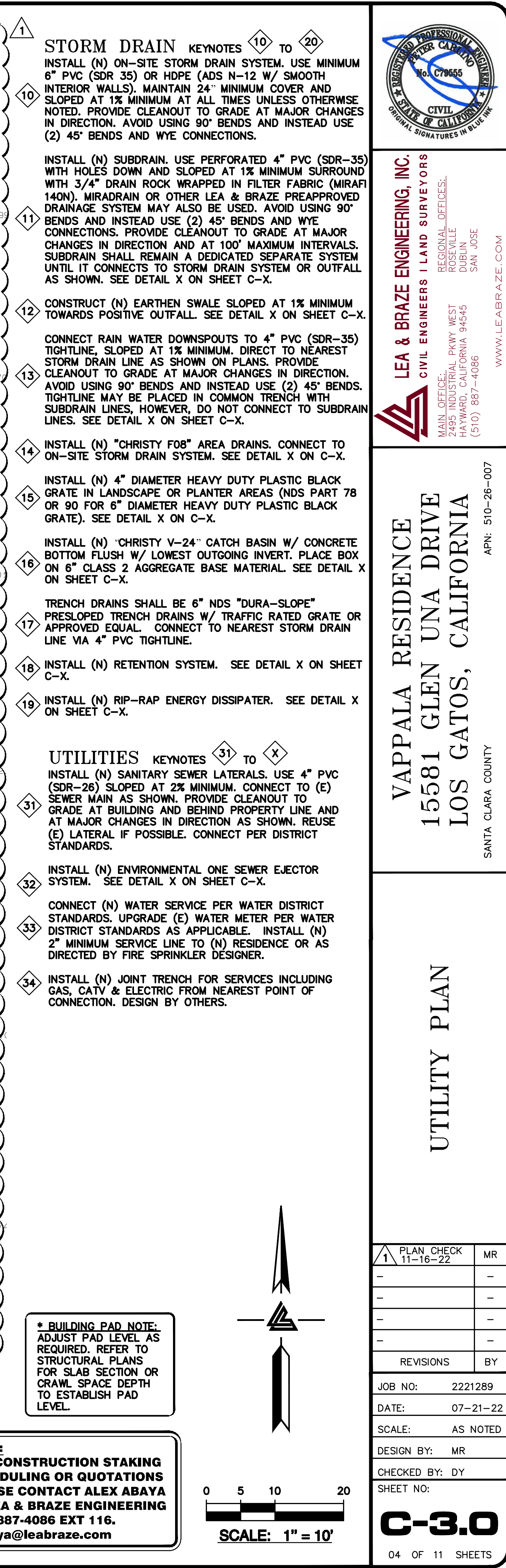


LEA & BRAZE ENGINEERING, INC.
CIVIL ENGINEERS & LAND SURVEYORS
REGIONAL OFFICES:
MAIN OFFICE: 15581 GLEN UNA DRIVE, LOS GATOS, CALIFORNIA 95056
SAN JOSE: (510) 887-4086
WWW.LEABRAZE.COM

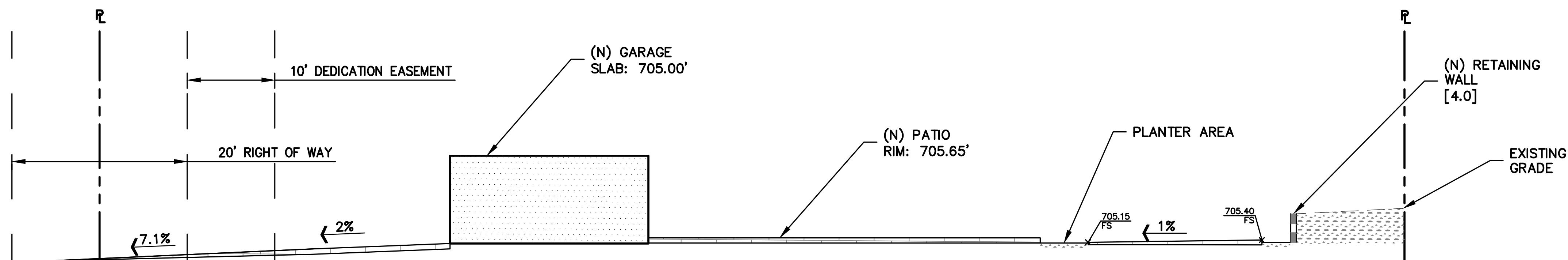
VAPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA
APN: 510-26-007
SANTA CLARA COUNTY

GRADING AND DRAINAGE PLAN

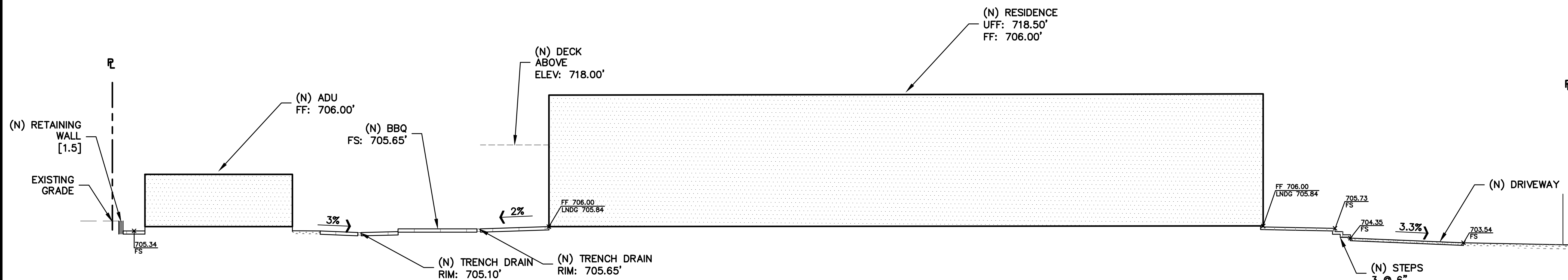
PLAN CHECK	MR
11-16-22	
REVISIONS	BY
JOB NO: 2221289	
DATE: 07-21-22	
SCALE: AS NOTED	
DESIGN BY: MR	
CHECKED BY: DY	
SHEET NO:	



04 OF 11 SHEETS



1 SECTION
C-4.0 NTS



2 SECTION
C-4.0 NTS



LEA & BRAZE ENGINEERING, INC.
CIVIL ENGINEERS & LAND SURVEYORS
REGIONAL OFFICES:
SAN JOSE OFFICE: 1000 SAN JOSE AVENUE, SUITE 100, SAN JOSE, CA 95128
SAN JOSE: (408) 887-4066
SAN JOSE: (408) 887-4066
WWW.LEABRAZE.COM

VAPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA
SANTA CLARA COUNTY
APN: 510-26-007

SITE SECTIONS

1	PLAN CHECK	MR
11-16-22		
REVISIONS	BY	
JOB NO:	2221289	
DATE:	07-21-22	
SCALE:	AS NOTED	
DESIGN BY:	MR	
CHECKED BY:	DY	
SHEET NO:		

C-4.0
05 OF 11 SHEETS

PURPOSE:

THE PURPOSE OF THIS PLAN IS TO STABILIZE THE SITE TO PREVENT EROSION OF GRADED AREAS AND TO PREVENT SEDIMENTATION FROM LEAVING THE CONSTRUCTION AREA AND AFFECTING NEIGHBORING SITES, NATURAL AREAS, PUBLIC FACILITIES OR ANY OTHER AREA THAT MIGHT BE AFFECTED BY SEDIMENTATION. ALL MEASURES SHOWN ON THIS PLAN SHOULD BE CONSIDERED THE MINIMUM REQUIREMENTS NECESSARY. SHOULD FIELD CONDITIONS DICTATE ADDITIONAL MEASURES, SUCH MEASURES SHALL BE PER CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL AND THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION. LEA & BRAZE ENGINEERING SHOULD BE NOTIFIED IMMEDIATELY SHOULD CONDITIONS CHANGE.

EROSION CONTROL NOTES:

- IT SHALL BE THE OWNER'S/CONTRACTOR'S RESPONSIBILITY TO MAINTAIN CONTROL OF THE ENTIRE CONSTRUCTION OPERATION AND TO KEEP THE ENTIRE SITE IN COMPLIANCE WITH THIS EROSION CONTROL PLAN.
- THE INTENTION OF THIS PLAN IS FOR INTERIM EROSION AND SEDIMENT CONTROL ONLY. ALL EROSION CONTROL MEASURES SHALL CONFORM TO CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL, THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION, AND THE LOCAL GOVERNING AGENCY FOR THIS PROJECT.
- OWNER/CONTRACTOR SHALL BE RESPONSIBLE FOR MONITORING EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO, DURING, AND AFTER STORM EVENTS. PERSON IN CHARGE OF MAINTAINING EROSION CONTROL MEASURES SHOULD WATCH LOCAL WEATHER REPORTS AND ACT APPROPRIATELY TO MAKE SURE ALL NECESSARY MEASURES ARE IN PLACE.
- SANITARY FACILITIES SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- DURING THE RAINY SEASON, ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIAL AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT ADEN RUNOFF TO ANY STORM DRAINAGE SYSTEM, INCLUDING EXISTING DRAINAGE SWALES AND WATERCOURSES.
- CONSTRUCTION OPERATIONS SHALL BE CARRIED OUT IN SUCH A MANNER THAT EROSION AND WATER POLLUTION WILL BE MINIMIZED. COMPLIANCE WITH FEDERAL, STATE AND LOCAL LAWS CONCERNING POLLUTION SHALL BE MAINTAINED AT ALL TIMES.
- CONTRACTOR SHALL PROVIDE DUST CONTROL AS REQUIRED BY THE APPROPRIATE FEDERAL, STATE AND LOCAL AGENCY REQUIREMENTS.
- ALL MATERIALS NECESSARY FOR THE APPROVED EROSION CONTROL MEASURES SHALL BE IN PLACE BY OCTOBER 15TH.
- EROSION CONTROL SYSTEMS SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE RAINY SEASON, OR FROM OCTOBER 15TH THROUGH APRIL 15TH, WHICHEVER IS LONGER.
- IN THE EVENT OF RAIN, ALL GRADING WORK IS TO CEASE IMMEDIATELY AND THE SITE IS TO BE SEALED IN ACCORDANCE WITH THE APPROVAL EROSION CONTROL MEASURES AND APPROVED EROSION CONTROL PLAN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING AND REPAIRING EROSION CONTROL SYSTEMS AFTER EACH STORM.
- ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BY LOCAL JURISDICTION'S ENGINEERING DEPARTMENT OR BUILDING OFFICIALS.
- MEASURES SHALL BE TAKEN TO COLLECT OR CLEAN ANY ACCUMULATION OR DEPOSIT OF DIRT, MUD, SAND, ROCKS, GRAVEL OR DEBRIS ON THE SURFACE OF ANY STREET, ALLEY OR PUBLIC PLACE OR IN ANY PUBLIC STORM DRAIN SYSTEMS. THE REMOVAL OF AFORESAID SHALL BE DONE BY STREET SWEEPING OR HAND SWEEPING. WATER SHALL NOT BE USED TO WASH SEDIMENTS INTO PUBLIC OR PRIVATE DRAINAGE FACILITIES.
- EROSION CONTROL MEASURES SHALL BE ON-SITE FROM SEPTEMBER 15TH THRU APRIL 15TH.
- ALL EROSION CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE RAINY SEASON OR FROM OCTOBER X THROUGH APRIL X, WHICHEVER IS GREATER.
- PLANS SHALL BE DESIGNED TO MEET C3 REQUIREMENTS OF THE MUNICIPAL STORMWATER REGIONAL PERMIT("MRP") NPDES PERMIT CAS 612008.
- THE CONTRACTOR TO NPDES (NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM) BEST MANAGEMENT PRACTICES (BMP) FOR SEDIMENTATION PREVENTION AND EROSION CONTROL TO PREVENT DELETERIOUS MATERIALS OR POLLUTANTS FROM ENTERING THE TOWN OR COUNTY STORM DRAIN SYSTEMS.
- THE CONTRACTOR MUST INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO THE INCEPTION OF ANY WORK ONSITE AND MAINTAIN THE MEASURES UNTIL THE COMPLETION OF ALL LANDSCAPING.
- THE CONTRACTOR SHALL MAINTAIN ADJACENT STREETS IN A NEAT, CLEAN DUST FREE AND SANITARY CONDITION AT ALL TIMES AND TO THE SATISFACTION OF THE TOWN INSPECTOR. THE ADJACENT STREET SHALL AT ALL TIMES BE KEPT CLEAN OF DEBRIS, WITH DUST AND OTHER NUISANCE BEING CONTROLLED AT ALL TIMES. THE CONTRACTOR BE RESPONSIBLE FOR ANY CLEAN UP ON ADJACENT STREETS AFFECTED BY THE BY THEIR CONSTRUCTION, METHOD OF STREET CLEANING SHALL BE BY DRY SWEEPING OF ALL PAVED AREAS. NO STOCKPILING OF BUILDING MATERIALS WITHIN THE TOWN RIGHT-OF-WAY.
- SEDIMENTS AND OTHER MATERIALS SHALL NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONTRACTOR SHALL INSTALL A STABILIZED CONSTRUCTION ENTRANCE PRIOR TO THE INSPECTION OF ANY WORK ONSITE AND MAINTAIN IT FOR THE DURATION OF THE CONSTRUCTION PROCESS SO AS TO NOT INHIBIT SEDIMENTS FROM BEING DEPOSITED INTO THE PUBLIC RIGHT-OF-WAY UNTIL THE COMPLETION OF ALL LANDSCAPING.
- THE CONTRACTOR SHALL PROTECT DOWN SLOPE DRAINAGE COURSES, STREAMS AND STORM DRAINS WITH ROCK FILLED SAND BAGS, TEMPORARY SWALES, SILT FENCES, AND EARTH PERMS IN CONJUNCTION OF ALL LANDSCAPING.
- STOCKPILED MATERIALS SHALL BE COVERED WITH VISQUEEN OR A TARPULIN UNTIL THE MATERIAL IS REMOVED FROM THE SITE. ANY REMAINING BARE SOIL THAT EXISTS AFTER THE STOCKPILE HAS BEEN REMOVED SHALL BE COVERED UNTIL A NATURAL GROUND COVER IS ESTABLISHED OR IT IS SEEDED OR PLANTED TO PROVIDE GROUND COVER PRIOR TO THE FALL RAINY SEASON.
- EXCESS OR WASTE CONCRETE MUST NOT BE WASHED INTO THE PUBLIC RIGHT-OF-WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTES ON SITE UNTIL THEY CAN BE DISPOSED OF AS SOLID WASTE.
- TRASH AND CONSTRUCTION RELATED SOLID WASTES MUST BE DEPOSITED INTO A COVERED RECEPTACLE TO PREVENT CONTAMINATION AND DISPERSAL BY WIND

EROSION CONTROL NOTES CONTINUED:

- FUELS, OILS, SOLVENTS AND OTHER TOXIC MATERIALS MUST BE STORED IN ACCORDANCE WITH THEIR LISTING AND ARE NOT TO CONTAMINATE THE SOIL AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM THE WEATHER. SPILLS MUST BE CLEANED UP IMMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MUST NOT BE WASHED INTO THE DRAINAGE SYSTEM,
- DUST CONTROL SHALL BE DONE BY WATERING AND AS OFTEN AS REQUIRED BY THE TOWN INSPECTOR.
- SILT FENCE(S) AND/OR FIBER ROLL(S) SHALL BE INSTALLED PRIOR TO SEPTEMBER 15TH AND SHALL REMAIN IN PLACE UNTIL THE LANDSCAPING GROUND COVER IS INSTALLED. CONTRACTOR SHALL CONTINUOUSLY MONITOR THESE MEASURES, FOLLOWING AND DURING ALL RAIN EVENTS, TO PUBLIC OWNED FACILITIES.

EROSION CONTROL MEASURES:

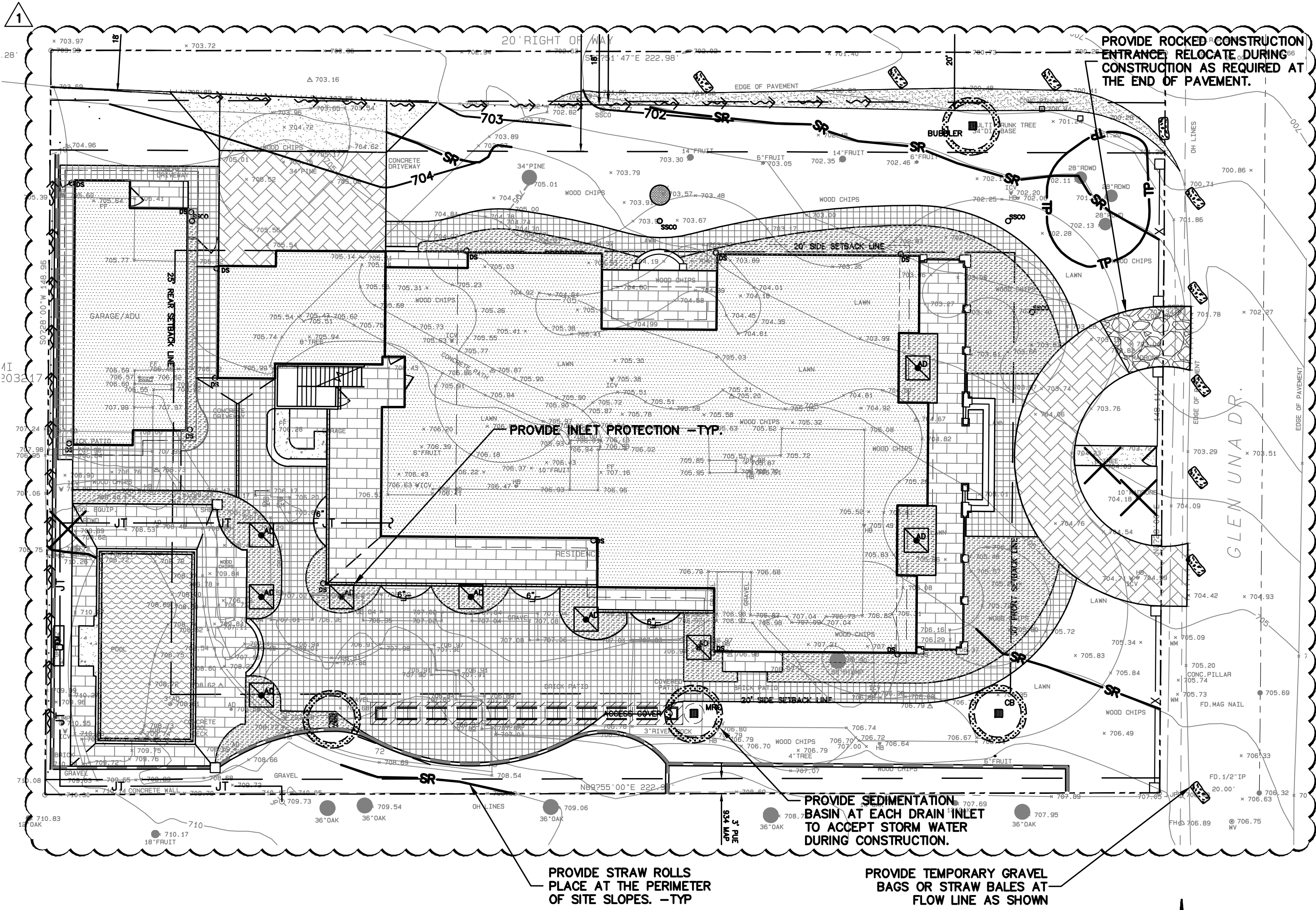
- THE FACILITIES SHOWN ON THIS PLAN ARE DESIGNED TO CONTROL EROSION AND SEDIMENT DURING THE RAINY SEASON, OCTOBER 15TH TO APRIL 15. EROSION CONTROL FACILITIES SHALL BE IN PLACE PRIOR TO OCTOBER 15TH OF ANY YEAR. GRADING OPERATIONS DURING THE RAINY SEASON WHICH LEAVE DENUDE SLOPES SHALL BE PROTECTED WITH EROSION CONTROL MEASURES IMMEDIATELY FOLLOWING GRADING ON THE SLOPES.
- SITE CONDITIONS AT TIME OF PLACEMENT OF EROSION CONTROL MEASURES WILL VARY. APPROPRIATE ACTION INCLUDING TEMPORARY SWALES, INLETS, HYDROSEEDING, STRAW BALES, ROCK SACKS, ETC. SHALL BE TAKEN TO PREVENT EROSION AND SEDIMENTATION FROM LEAVING SITE. EROSION CONTROL MEASURES SHALL BE ADJUSTED AS THE CONDITIONS CHANGE AND THE NEED OF CONSTRUCTION SHIFT.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF GRADING. ALL CONSTRUCTION TRAFFIC ENTERING ONTO THE PAVED ROADS MUST CROSS THE STABILIZED CONSTRUCTION ENTRANCES. CONTRACTOR SHALL MAINTAIN STABILIZED ENTRANCE AT EACH VEHICLE ACCESS POINT TO EXISTING PAVED STREETS. ANY MUD OR DEBRIS TRACKED ONTO PUBLIC STREETS SHALL BE REMOVED DAILY AND AS REQUIRED BY THE GOVERNING AGENCY.
- ALL EXPOSED SLOPES THAT ARE NOT VEGETATED SHALL BE HYDROSEEDDED, IF HYDROSEEDING IS NOT USED OR IS NOT EFFECTIVE BY OCTOBER 15, THEN OTHER IMMEDIATE METHODS SHALL BE IMPLEMENTED, SUCH AS EROSION CONTROL BLANKETS, OR A THREE-STEP APPLICATION OF 1) SEED, MULCH, FERTILIZER 2) BLOWN STRAW 3) TACKIFIER AND MULCH. HYDROSEEDING SHALL BE IN ACCORDANCE WITH THE PROVISIONS OF SECTION 20" EROSION CONTROL AND HIGHWAY PLANTING" OF THE STANDARD SPECIFICATION OF THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION, AS LAST REVISED. REFER TO THE EROSION CONTROL SECTION OF THE GRADING SPECIFICATIONS THAT ARE A PART OF THIS PLAN SET FOR FURTHER INFORMATION.
- INLET PROTECTION SHALL BE INSTALLED AT OPEN INLETS TO PREVENT SEDIMENT FROM ENTERING THE STORM DRAIN SYSTEM. INLETS NOT USED IN CONJUNCTION WITH EROSION CONTROL ARE TO BE BLOCKED TO PREVENT ENTRY OF SEDIMENT. MINIMUM INLET PROTECTION SHALL CONSIST OF A ROCK SACKS OR AS SHOWN ON THIS PLAN
- THIS EROSION AND SEDIMENT CONTROL PLAN MAY NOT COVER ALL THE SITUATIONS THAT MAY ARISE DURING CONSTRUCTION DUE TO UNANTICIPATED FIELD CONDITIONS. VARIATIONS AND ADDITIONS MAY BE MADE TO THIS PLAN IN THE FIELD. A REPRESENTATIVE OF LEA & BRAZE ENGINEERING SHALL PERFORM A FIELD REVIEW AND MAKE RECOMMENDATIONS AS NEEDED. CONTRACTOR IS RESPONSIBLE TO NOTIFY LEA & BRAZE ENGINEERING AND THE GOVERNING AGENCY OF ANY CHANGES.
- THE EROSION CONTROL MEASURES SHALL CONFORM TO THE LOCAL JURISDICTION'S STANDARDS AND THE APPROVAL OF THE LOCAL JURISDICTION'S ENGINEERING DEPARTMENT.
- STRAW ROLLS SHALL BE PLACED AT THE TOE OF SLOPES AND ALONG THE DOWN SLOPE PERIMETER OF THE PROJECT. THEY SHALL BE PLACED AT 25 FOOT INTERVALS ON GRADED SLOPES. PLACEMENT SHALL RUN WITH THE CONTOURS AND ROLLS SHALL BE TIGHTLY END BUTTED. CONTRACTOR SHALL REFER TO MANUFACTURES SPECIFICATIONS FOR PLACEMENT AND INSTALLATION INSTRUCTIONS.

REFERENCES:

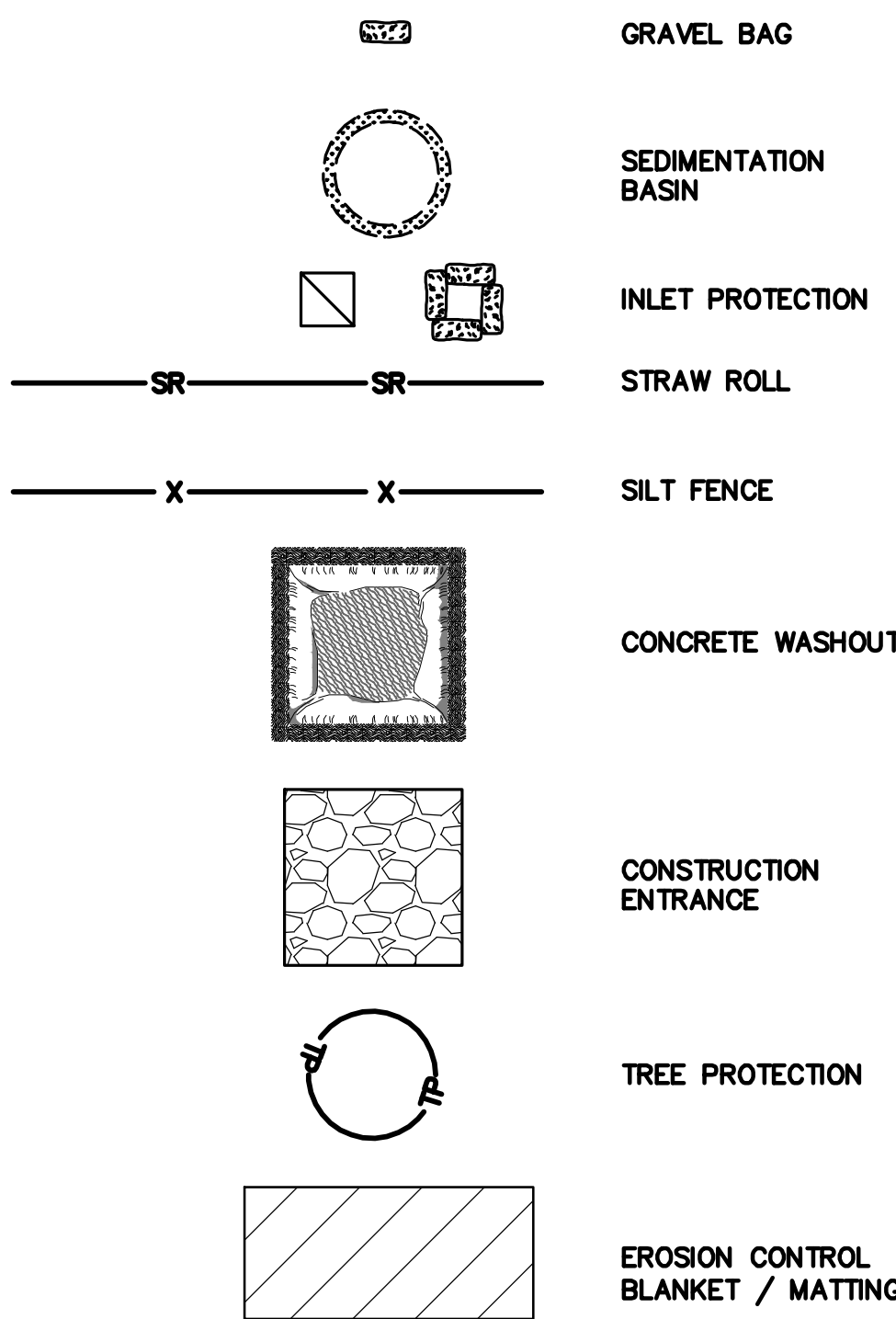
- CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL
- CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION

PERIODIC MAINTENANCE:

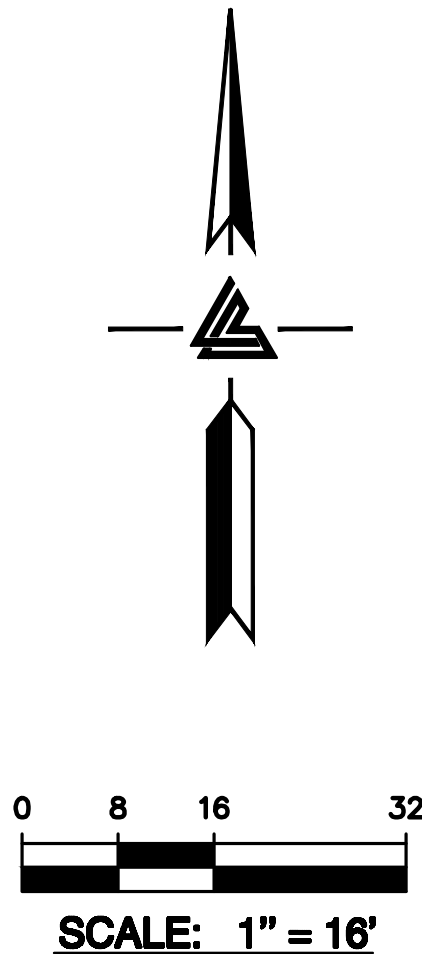
- MAINTENANCE IS TO BE PERFORMED AS FOLLOWS:
 - DAMAGES CAUSED BY SOIL EROSION OR CONSTRUCTION SHALL BE REPAIRED AT THE END OF EACH WORKING DAY.
 - SWALES SHALL BE INSPECTED PERIODICALLY AND MAINTAINED AS NEEDED.
 - SEDIMENT TRAPS, BERMS, AND SWALES ARE TO BE INSPECTED AFTER EACH STORM AND REPAIRS MADE AS NEEDED.
 - SEDIMENT SHALL BE REMOVED AND SEDIMENT TRAP RESTORED TO ITS ORIGINAL DIMENSIONS WHEN SEDIMENT HAS ACCUMULATED TO A DEPTH OF 1" FOOT.
 - SEDIMENT REMOVED FROM TRAP SHALL BE DEPOSITED IN A SUITABLE AREA AND IN SUCH A MANNER THAT IT WILL NOT ERODE.
 - RILLS AND GULLIES MUST BE REPAIRED.
- GRAVEL BAG INLET PROTECTION SHALL BE CLEANED OUT WHENEVER SEDIMENT DEPTH IS ONE HALF THE HEIGHT OF ONE GRAVEL BAG.
- STRAW ROLLS SHALL BE PERIODICALLY CHECKED TO ASSURE PROPER FUNCTION AND CLEANED OUT WHENEVER THE SEDIMENT DEPTH REACHED HALF THE HEIGHT OF THE ROLL.
- SILT FENCE SHALL BE PERIODICALLY CHECKED TO ASSURE PROPER FUNCTION AND CLEANED OUT WHENEVER THE SEDIMENT DEPTH REACHES ONE FOOT IN HEIGHT.
- CONSTRUCTION ENTRANCE SHALL BE REGRAVELED AS NECESSARY FOLLOWING SILT/SOIL BUILDUP.
- ANY OTHER EROSION CONTROL MEASURES SHOULD BE CHECKED AT REGULAR INTERVALS TO ASSURE PROPER FUNCTION



EROSION CONTROL LEGEND



NOTE:
SEAL ALL OTHER INLETS NOT INTENDED TO ACCEPT STORM WATER AND DIRECT FLOWS TEMPORARILY TO FUNCTIONAL SEDIMENTATION BASIN INLETS. -TYP



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CIVIL ENGINEERS & LAND SURVEYORS
REGIONAL OFFICES:
MAIN OFFICE: 15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA 95055
(510) 887-4086
WWW.LEABRAZE.COM

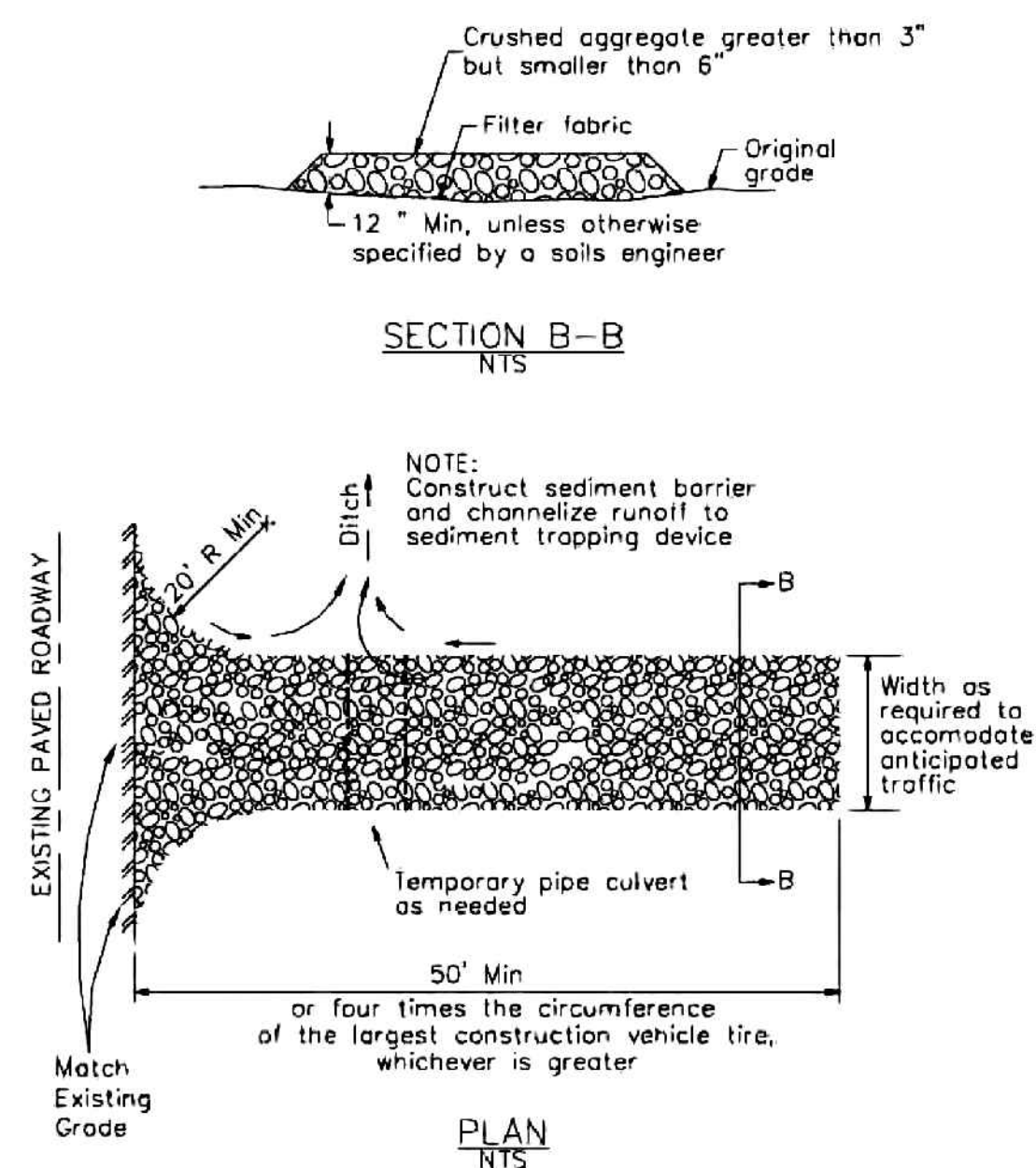
VAPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA

EROSION CONTROL
PLAN

PLAN CHECK	MR
11-16-22	
REVISIONS	BY
JOB NO:	2221289
DATE:	07-21-22
SCALE:	AS NOTED
DESIGN BY:	MR
CHECKED BY:	DY
SHEET NO:	

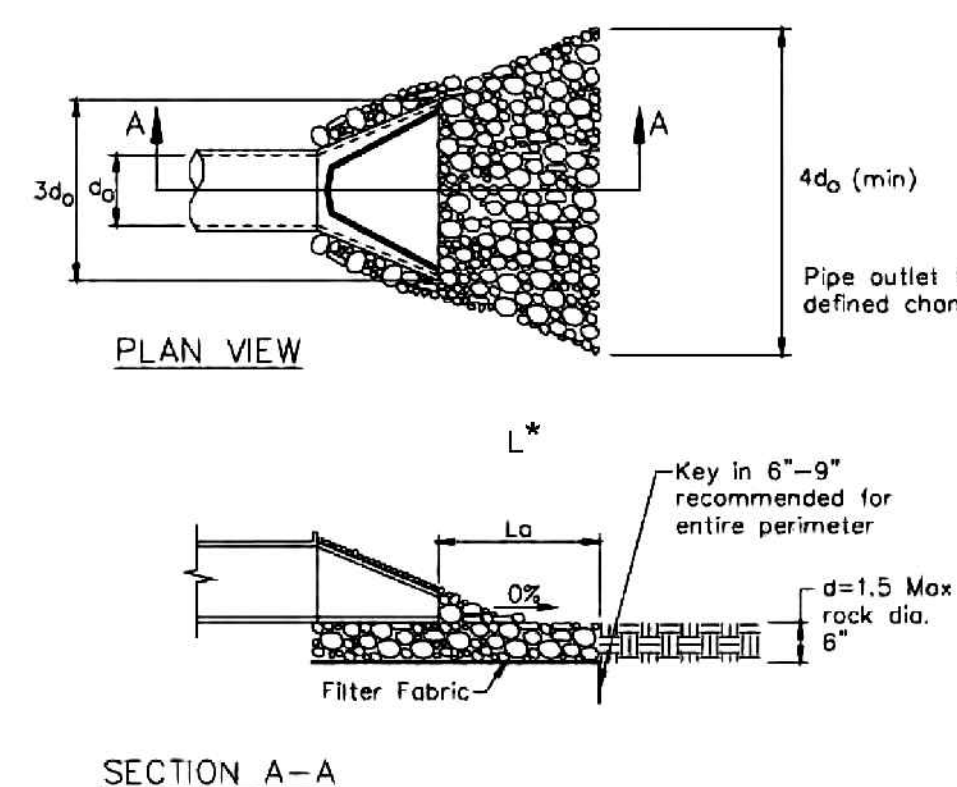
3 Stabilized Construction Entrance/Exit

CASQA Detail TC-1



4 Velocity Dissipation Devices

CASQA Detail EC-10

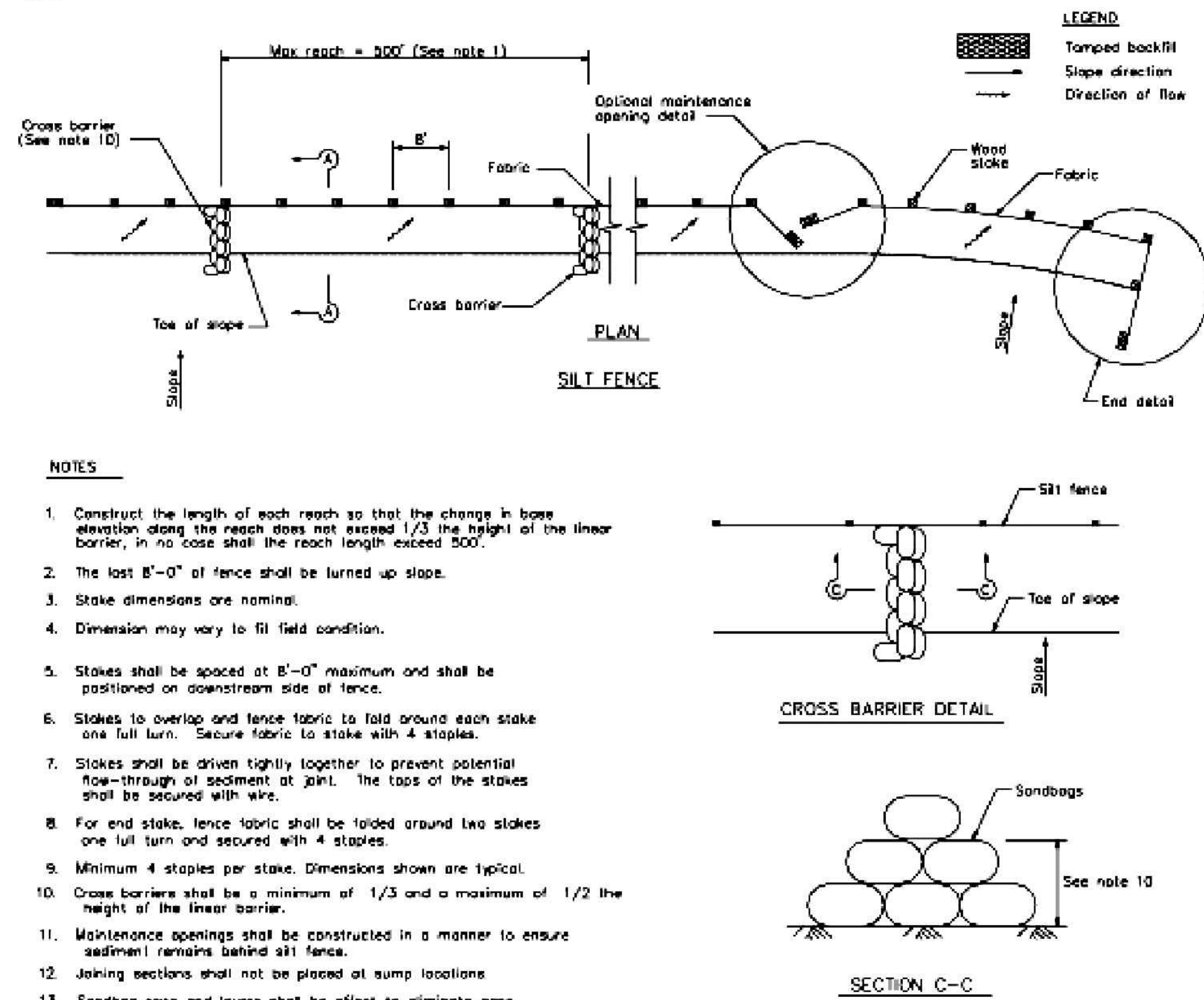


* Length per ABAG Design Standards

Source for Graphics: California Stormwater BMP Handbook, California Stormwater Quality Association, January 2003.
Available from www.cabmphandbooks.com.

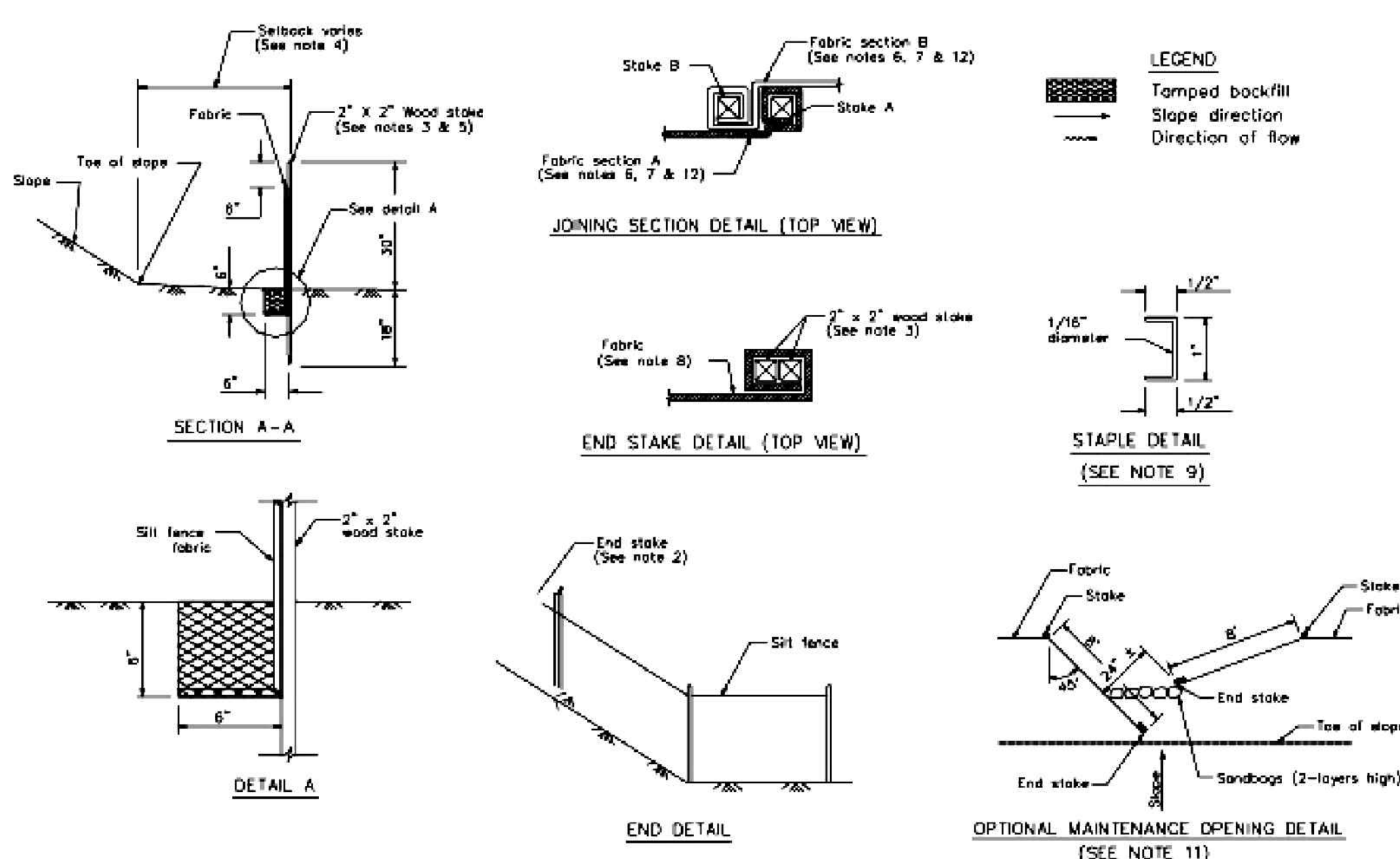
Silt Fence

CASQA Detail SE-1



Silt Fence

CASQA Detail SE-1



STANDARD BEST MANAGEMENT PRACTICE NOTES

- Solid and Demolition Waste Management:** Provide designated waste collection areas and containers on site away from streets, gutters, storm drains, and waterways, and arrange for regular disposal. Waste containers must be watertight and covered at all times except when waste is deposited. Refer to Erosion & Sediment Control Field Manual, 4th Edition (page C3) or latest.
- Hazardous Waste Management:** Provide proper handling and disposal of hazardous wastes by a licensed hazardous waste material hauler. Hazardous wastes shall be stored and properly labeled in sealed containers constructed of suitable materials. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-5 to C-6) or latest.
- Spill Prevention and Control:** Provide proper storage areas for liquid and solid materials, including chemicals and hazardous substances, away from streets, gutters, storm drains, and waterways. Spill control materials must be kept on site where readily accessible. Spills must be cleaned up immediately and contaminated soil disposed properly. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-7 to C-8, C-13 to C-14) or latest.
- Vehicle and Construction Equipment Service and Storage:** An area shall be designated for the maintenance, where on-site maintenance is required, and storage of equipment that is protected from stormwater run-on and runoff. Measures shall be provided to capture any waste oils, lubricants, or other potential pollutants and these wastes shall be properly disposed of off site. Fueling and major maintenance/repair, and washing shall be conducted off-site whenever feasible. Refer to Erosion & Sediment Control Field Manual, 4th Edition (page C9) or latest.
- Material Delivery, Handling and Storage:** In general, materials should not be stockpiled on site. Where temporary stockpiles are necessary and approved by the County, they shall be covered with secured plastic sheeting or tarp and located in designated areas near construction entrances and away from drainage paths and waterways. Barriers shall be provided around storage areas where materials are potentially in contact with runoff. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-11 to C-12) or latest.
- Handling and Disposal of Concrete and Cement:** When concrete trucks and equipment are washed on-site, concrete wastewater shall be contained in designated containers or in a temporary lined and watertight pit where wasted concrete can harden for later removal. If possible have concrete contractor remove concrete wash water from site. In no case shall fresh concrete be washed into the road right-of-way. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-15 to C-16) or latest.
- Pavement Construction Management:** Prevent or reduce the discharge of pollutants from paving operations, using measures to prevent run-on and runoff pollution and properly disposing of wastes. Avoid paving in the wet season and reschedule paving when rain is in the forecast. Residue from saw-cutting shall be vacuumed for proper disposal. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-17 to C-18) or latest.
- Contaminated Soil and Water Management:** Inspections to identify contaminated soils should occur prior to construction and at regular intervals during construction. Remediating contaminated soil should occur promptly after identification and be specific to the contaminant identified, which may include hazardous waste removal. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages C-19 to C-20) or latest.
- Sanitary/Septic Water Management:** Temporary sanitary facilities should be located away from drainage paths, waterways, and traffic areas. Only licensed sanitary and septic waste haulers should be used. Secondary containment should be provided for all sanitary facilities. Refer to Erosion & Sediment Control Field Manual, 4th Edition (page C-21) or latest.
- Inspection & Maintenance:** Areas of material and equipment storage sites and temporary sanitary facilities must be inspected weekly. Problem areas shall be identified and appropriate additional and/or alternative control measures implemented immediately, within 24 hours of the problem being identified.

STANDARD EROSION CONTROL NOTES

- Sediment Control Management:**
 - Tracking Prevention & Clean Up:** Activities shall be organized and measures taken as needed to prevent or minimize tracking of soil onto the public street system. A gravel or proprietary device construction entrance/exit is required for all sites. Clean up of tracked material shall be provided by means of a street sweeper prior to an approaching rain event, or at least once at the end of each workday that material is tracked, or, more frequently as determined by the County Inspector. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages B-31 to B-33) or latest.
 - Storm Drain Inlet and Catch Basin Inlet Protection:** All inlets within the vicinity of the project and within the project limits shall be protected with gravel bags placed around inlets or other inlet protection. At locations where exposed soils are present, staked fiber roles or staked silt fences can be used. Inlet filters are not allowed due to clogging and subsequent flooding. Refer to Erosion & Sediment Control Field Manual, 4th Edition (pages B-49 to B-51) or latest.
 - Storm Water Runoff:** No storm water runoff shall be allowed to drain in to the existing and/or proposed underground storm drain system or other above ground watercourses until appropriate erosion control measures are fully installed.
 - Dust Control:** The contractor shall provide dust control in graded areas as required by providing wet suppression or chemical stabilization of exposed soils, providing for rapid clean up of sediments deposited on paved roads, furnishing construction road entrances and vehicle wash down areas, and limiting the amount of areas disturbed by clearing and earth moving operations by scheduling these activities in phases.
 - Stockpiling:** Excavated soils shall not be placed in streets or on paved areas. Borrow and temporary stockpiles shall be protected with appropriate erosion control measures (tarps, straw bales, silt fences, ect.) to ensure silt does not leave the site or enter the storm drain system or neighboring watercourse.
- Erosion Control:** During the rainy season, all disturbed areas must include an effective combination of erosion and sediment control. It is required that temporary erosion control measures are applied to all disturbed soil areas prior to a rain event. During the non-rainy season, erosion control measures must be applied sufficient to control wind erosion at the site.
- Inspection & Maintenance:** Disturbed areas of the Project's site, locations where vehicles enter or exit the site, and all erosion and sediment controls that are identified as part of the Erosion Control Plans must be inspected by the Contractor before, during, and after storm events, and at least weekly during seasonal wet periods. Problem areas shall be identified and appropriate additional and/or alternative control measures implemented immediately, within 24 hours of the problem being identified.
- Project Completion:** Prior to project completion and signoff by the County Inspector, all disturbed areas shall be reseeded, planted, or landscaped to minimize the potential for erosion on the subject site.
- It shall be the Owner's/Contractor's responsibility to maintain control of the entire construction operation and to keep the entire site in compliance with the erosion control plan.
- Erosion and sediment control best management practices shall be operable year round or until vegetation is fully established on landscaped surfaces.

Project Information



LEA & BRAZE ENGINEERING, INC.
CIVIL ENGINEERS & LAND SURVEYORS
REGIONAL OFFICES:
MAIN OFFICE: 15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA 95056
(510) 887-4066
WWW.LEABRAZE.COM

VAPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA

APN: 510-26-007

SANTA CLARA COUNTY

BEST MANAGEMENT
PRACTICES

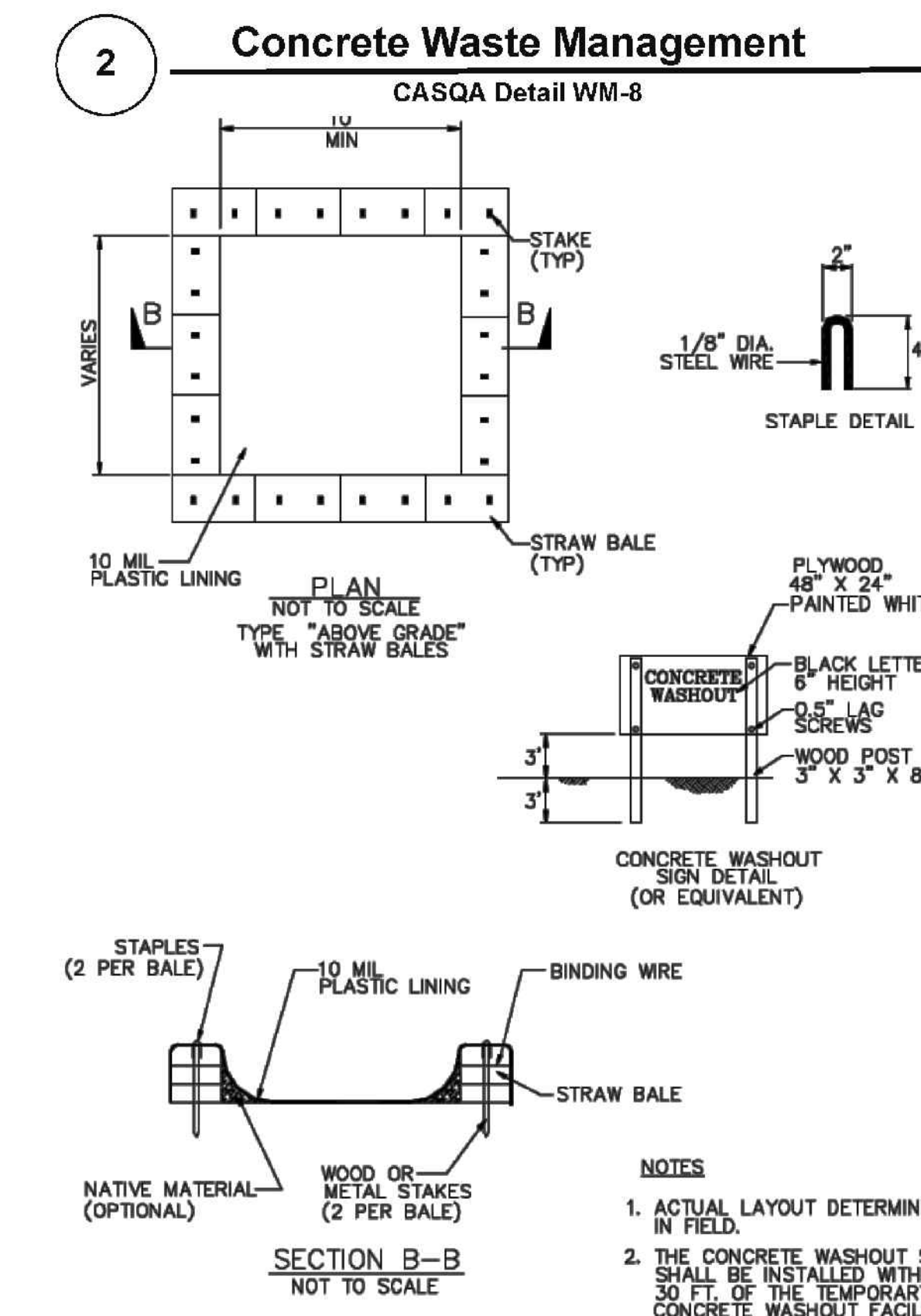
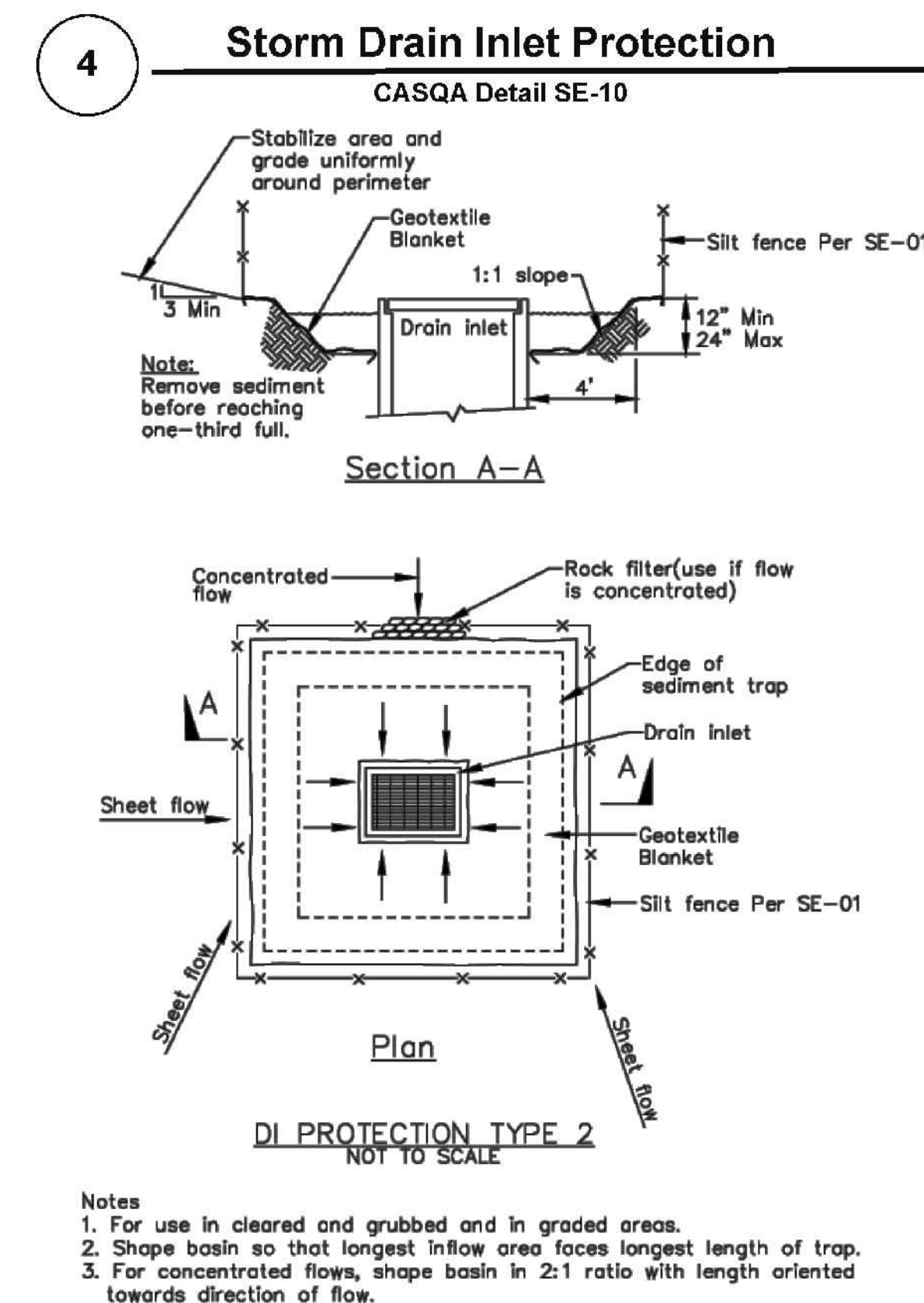
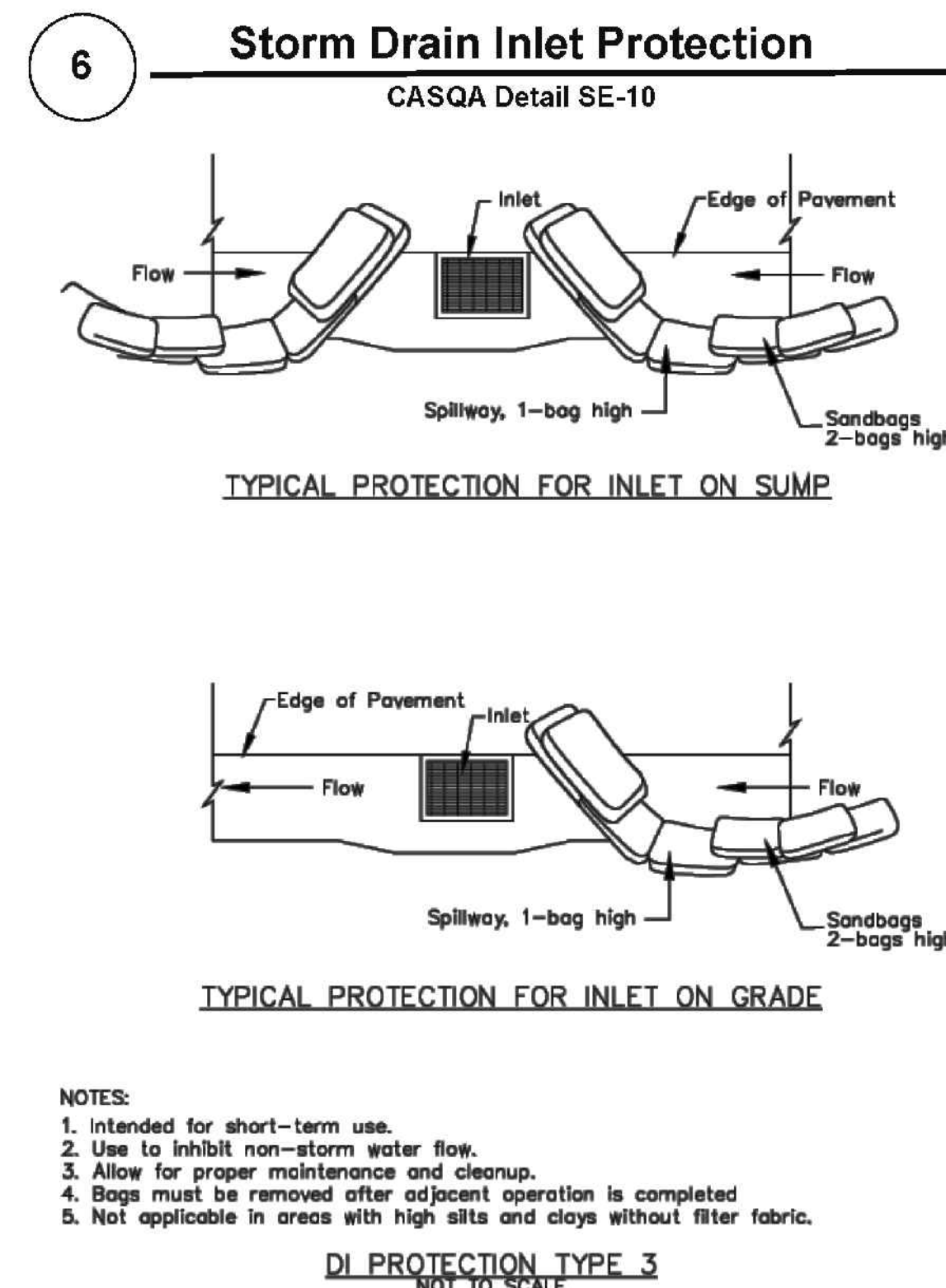
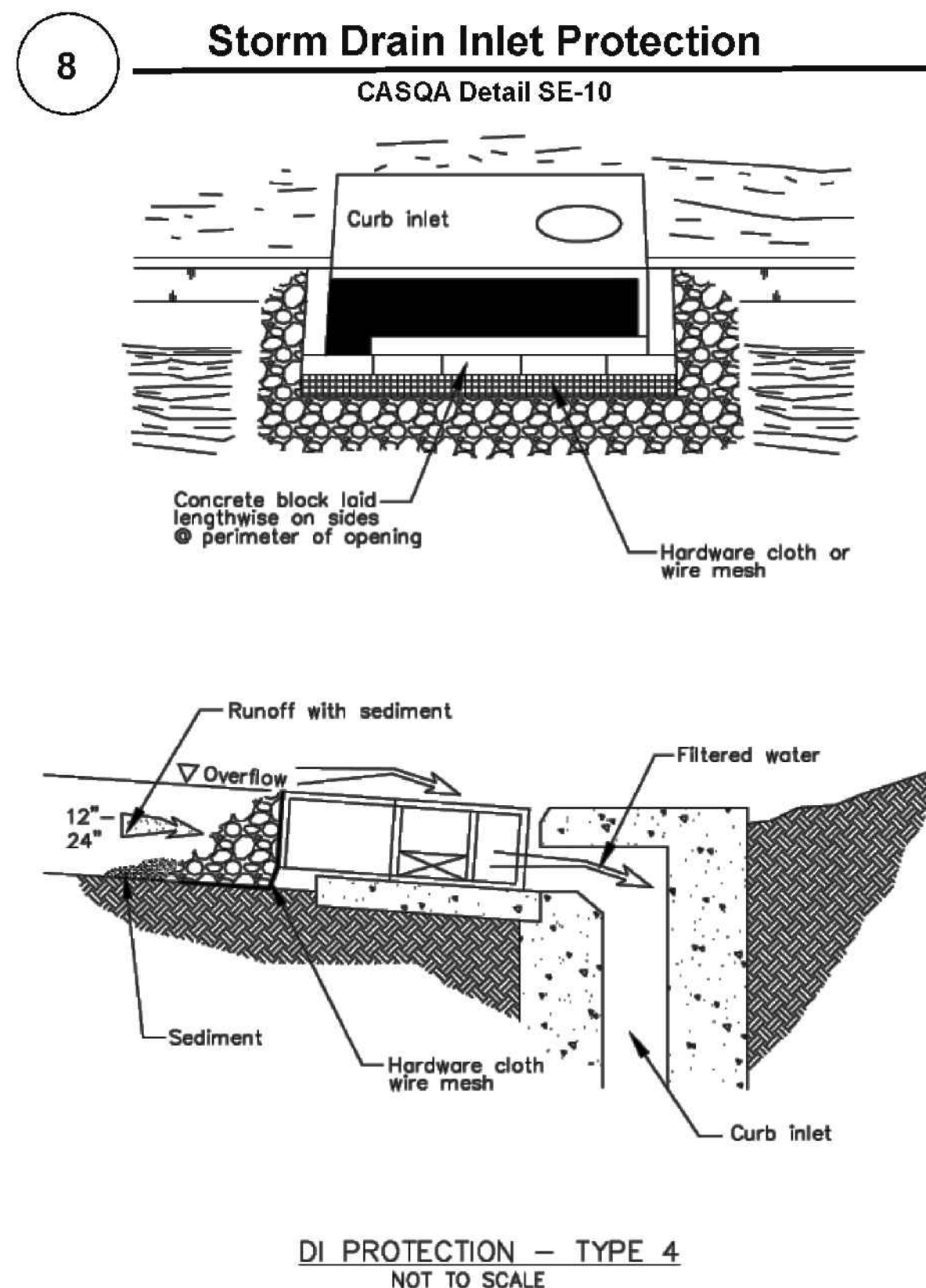
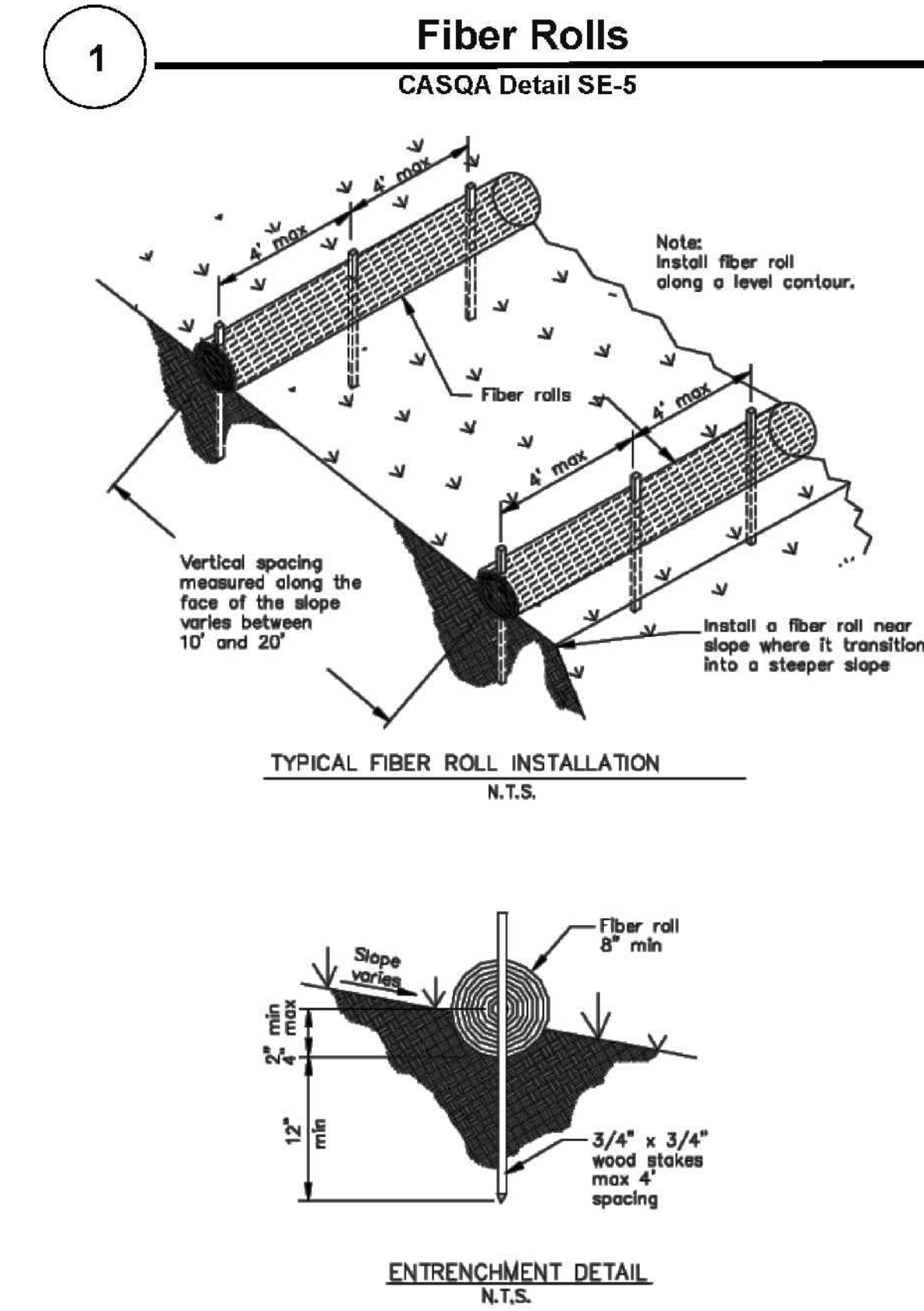
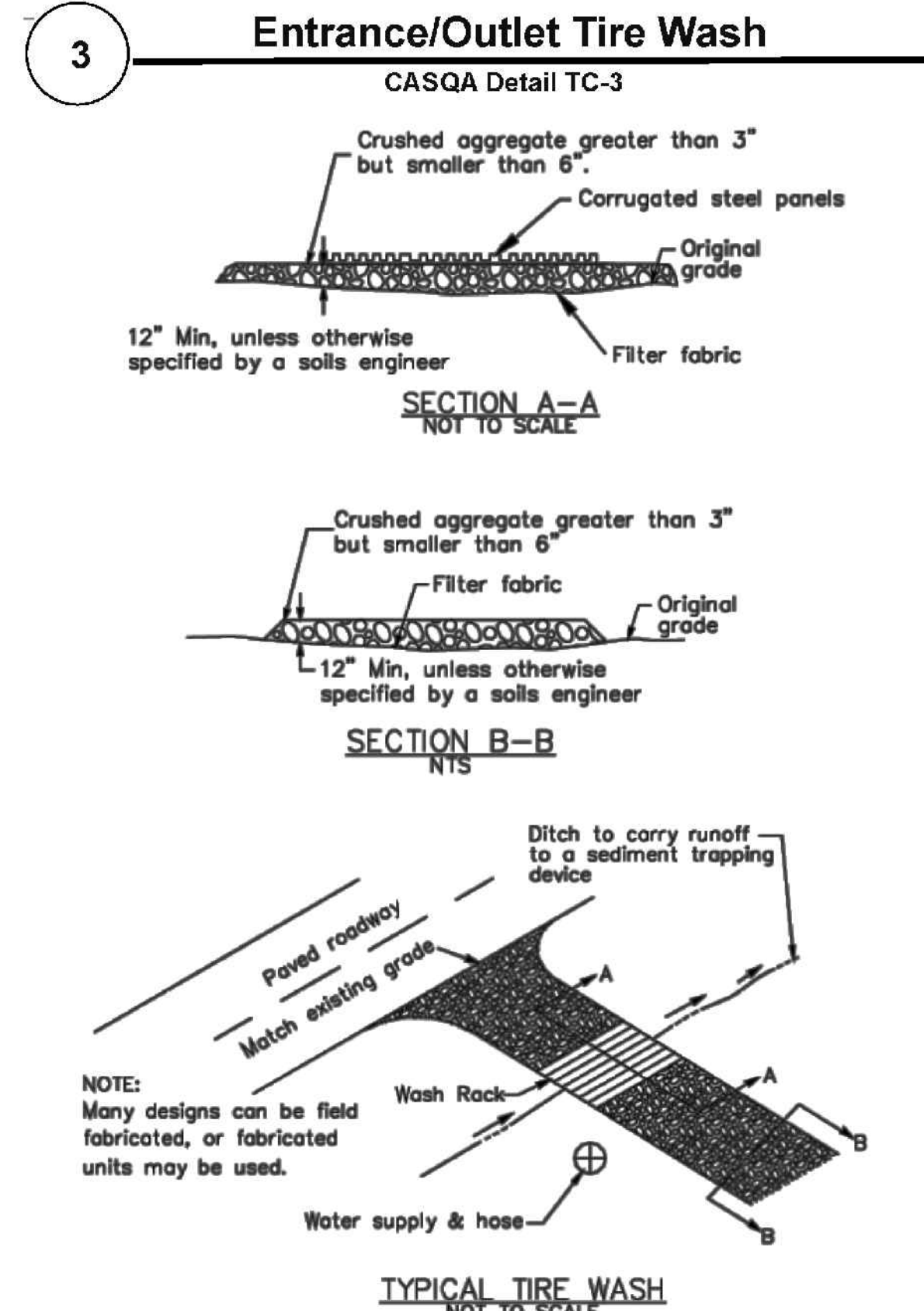
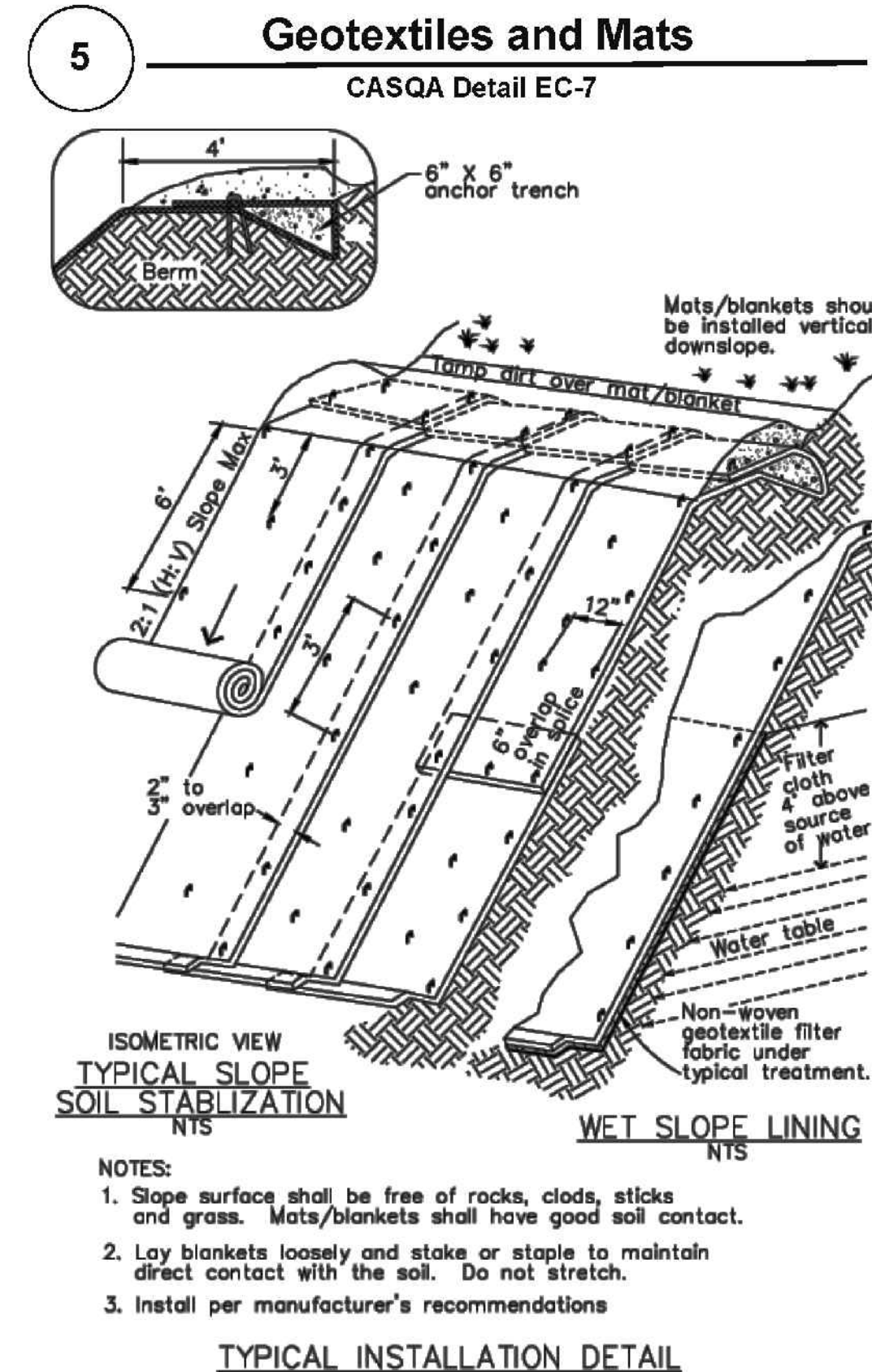
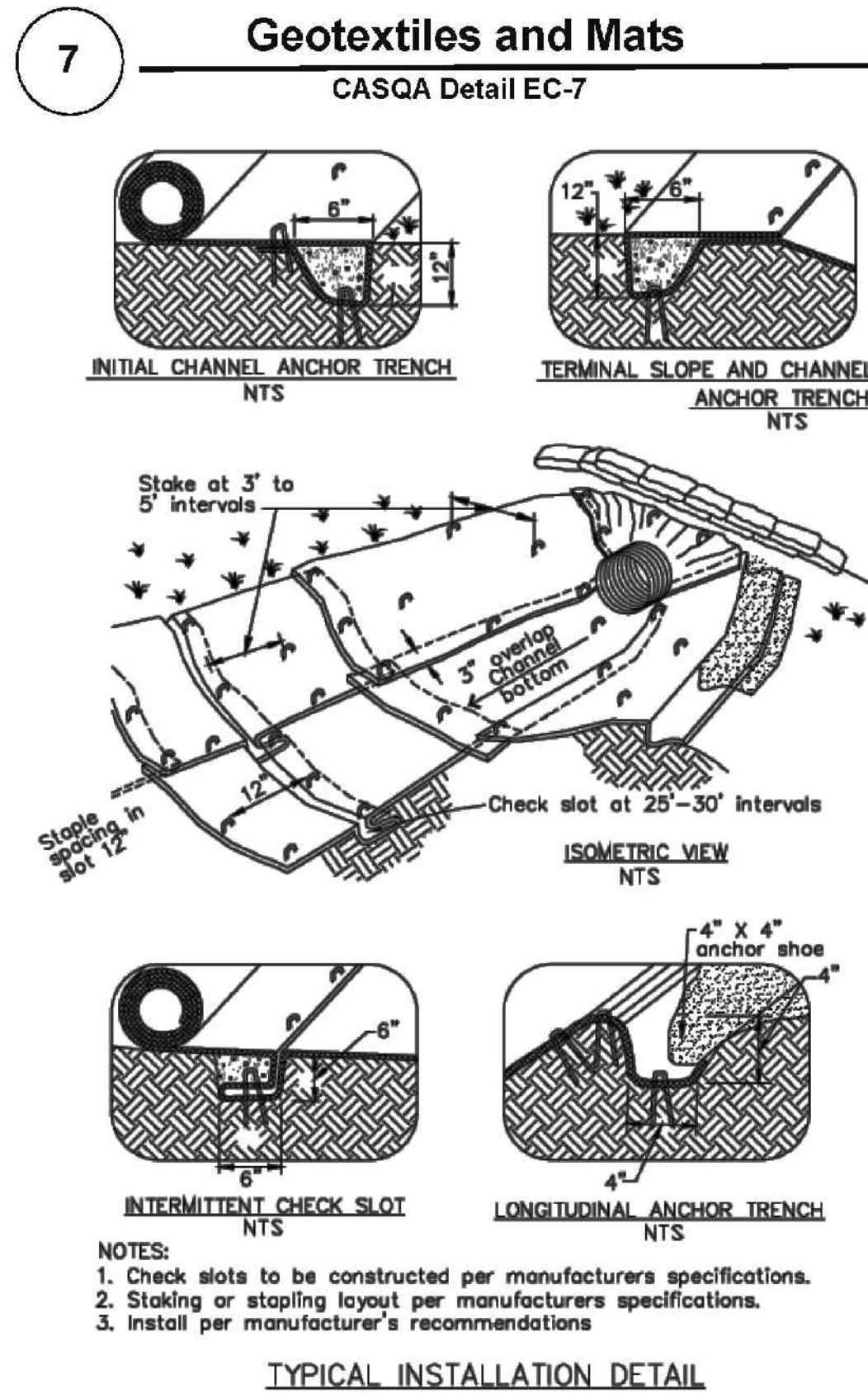
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11-16-22	
REVISIONS	BY
JOB NO: 2221289	
DATE: 07-21-22	
SCALE: AS NOTED	
DESIGN BY: MR	
CHECKED BY: DY	
SHEET NO:	

BMP-1
07 OF 11 SHEETS

Best Management Practices and Erosion Control Details Sheet 1
County of Santa Clara



BMP-1



Source for Graphics: California Stormwater BMP Handbook, California Stormwater Quality Association, January 2003.
Available from www.cabmphandbooks.com.

Best Management Practices and Erosion Control Details Sheet 2

County of Santa Clara



BMP-2



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CIVIL ENGINEERS & LAND SURVEYORS
REGIONAL OFFICES:
SAN JOSE, CALIFORNIA 95128
SAN JOSE, CALIFORNIA 95128
(415) 887-4066
WWW.LEABRAZE.COM

VAPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA

APN: 510-26-007

SANTA CLARA COUNTY

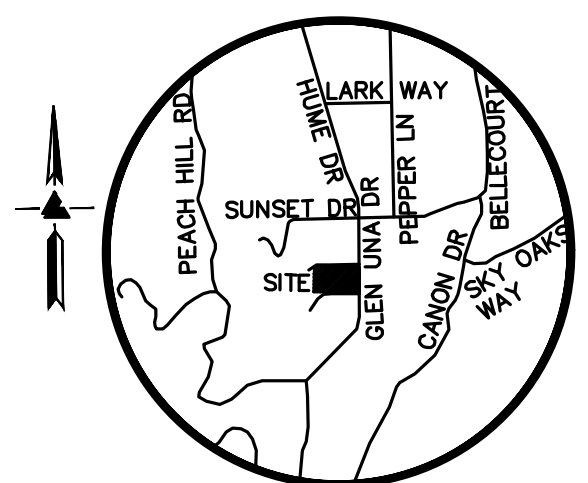
BEST MANAGEMENT
PRACTICES

Project Information

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11-16-22	
REVISIONS	BY
JOB NO: 2221289	
DATE: 07-21-22	
SCALE: AS NOTED	
DESIGN BY: MR	
CHECKED BY: DY	
SHEET NO:	

BMP-2
08 OF 11 SHEETS

AVERAGE LOT SLOPE EXHIBIT
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA



VICINITY MAP
NO SCALE



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REGIONAL OFFICES:
SAN JOSE, CA
DUBLIN, CA
SAN JOSE, CA
(510) 887-4086
WWW.LEABRAZE.COM

VAPPALA RESIDENCE
15581 GLEN UNA DRIVE
LOS GATOS, CALIFORNIA
SANTA CLARA COUNTY
APN: 51-26-007

AVERAGE LOT SLOPE
EXHIBIT

REVISIONS	BY
JOB NO:	2221289
DATE:	07-21-22
SCALE:	1"=16'
EXHIBIT DRAWN BY:	RW
CALCULATIONS BY:	RW
SHEET NO:	SL-1
1	OF 1 SHEETS

AREA WITHIN RIGHT-OF-WAY
2,181 SQFT. (0.005 ACRE)
(EXCLUDED FROM CALCULATIONS)

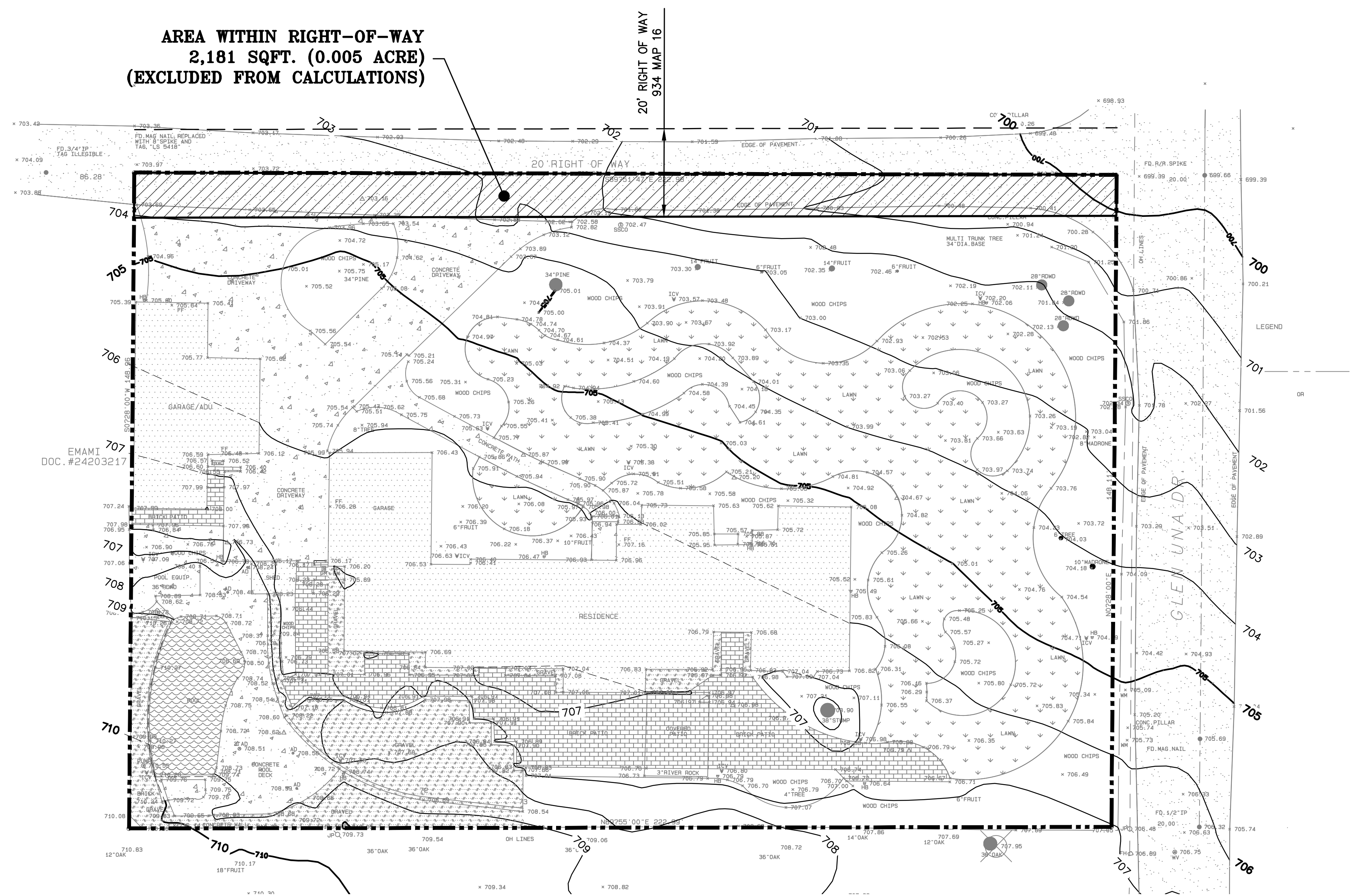


EXHIBIT NOTE

THIS EXHIBIT IS INTENDED ONLY AS AN AID TO DETERMINE THE AVERAGE LOT SLOPE OF THE SUBJECT PROPERTY. SITE SURVEY INFORMATION, INCLUDING BUT NOT LIMITED TO, UTILITIES, FENCES AND SPECIFIC SPOT GRADES MAY BE REMOVED FOR CLARITY. REFER TO THE SITE SURVEY BY ROBERT J. CRAIG FOR COMPLETE SITE SURVEY INFORMATION

CALCULATIONS BY LEA & BRAZE ENGINEERING (510) 887-4086

CALCULATION OF AVERAGE SLOPE

PROPERTY OWNER(S)			
PROPERTY ADDRESS		15581 Glen Una Drive	
CALCULATED BY		DATE	July 19, 2022
REFERENCE MAP:		JOB#	2221289

1. CALCULATION OF AVERAGE SLOPE

ZONING	N/A
GROSS AREA	0.76 ACRE (33,120 sqft.)
AREA IN R.O.W.	0.05 ACRE (2,181 sqft.) (Excluded from calculations)

A. NET AREA (An) 0.71 ACRES B. CONTOUR INTERVAL (I) 1.0 FT.

C. DRAWING SCALE 1"=16'

D. CONTOUR LENGTH WITHIN NET AREA OF LOT (An)

CONTOUR	LENGTH (INCHES)	CONTOUR	LENGTH (INCHES)	CONTOUR	LENGTH (INCHES)	CONTOUR	LENGTH (INCHES)
699	0.0	711	0.0	723		735	
700	0.0	712	0.0	724		736	
701	3.7	713		725		737	
702	7.0	714		726		738	
703	9.8	715		727		739	
704	14.8	716		728		740	
705	15.7	717		729		741	
706	15.5	718		730		742	
707	29.3	719		731		743	
708	12.0	720		732		744	
709	9.3	721		733		745	
710	1.7	722		734		746	
TOTAL						118.8	

CONVERT INCHES TO FEET (MULTIPLY BY MAP SCALE) = (L) = 1900.8 FT.

E. AVERAGE SLOPE WITHIN NET AREA OF LOT

S= (0.00229) (I) (L) (A)

S= (0.00229) (1 FT) (1900.8 FT.) = 6.1% (0.71 ACRES)

SURVEY INFORMATION

A REQUESTED TITLE REPORT WAS NOT PROVIDED FOR THIS SURVEY.

ROBERT J. CRAIG LICENSED LAND SURVEYOR 966 ELSIE MAE DRIVE BOULDER CREEK, CA 95006 (831)359-1750 OR (408)884-3791 RobertJamesCraig@gmail.com		TOPOGRAPHIC MAP OF LANDS OF VAPPALA & JALADI AS RECORDED IN DOCUMENT NO. 24244049 OFFICIAL RECORDS OF SANTA CLARA CO. Situate in LOS GATOS Unincorporated area of County of Santa Clara, State of California	
DATE: JANUARY 23, 2020	DRAWN: RJC	JOB NO. C-19143	INDEX: SANTA CLARA CO.
FIELDWORK: NIB	APN 510-26-007		

TECHNICAL INFORMATION

ASSESSOR'S PARCEL NUMBER: 510-26-007

ADDRESS OF PROJECT: 15581 GLEN UNA
LOS GATOS, CA

OWNERS' NAMES: LINGA VAPPALA
KALYANI JALADI

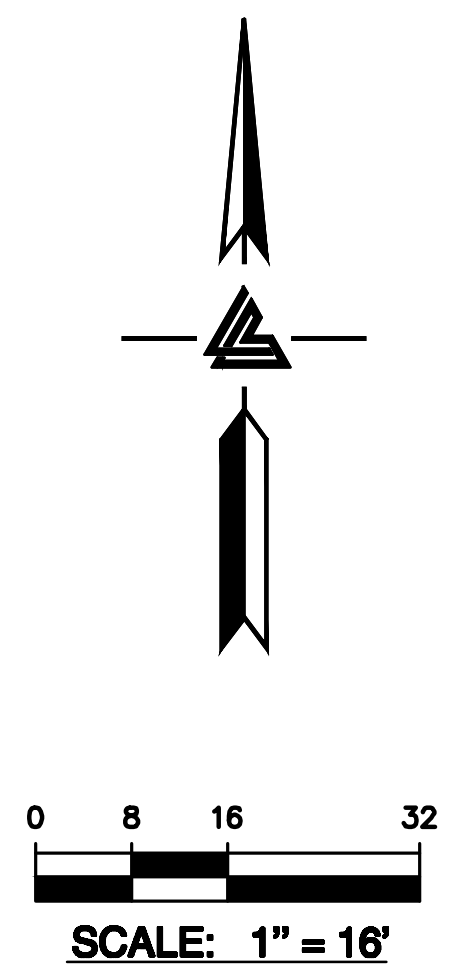
SIZE OF LOT: 33,120 SQ FT. +/- (GROSS)
30,890 SQ FT. +/- (NET)

BASIS OF BEARINGS

THE BEARINGS SHOWN ON THIS MAP ARE BASED ON THE CENTER LINE OF GLEN UNA DRIVE AS FOUND MONUMENTED AND RECORDED AS NORTH 0°28'00"EAST ON THE RECORD OF SURVEY FILED IN BOOK 403 OF MAPS, PAGE 26, SANTA CLARA COUNTY RECORDS.

BASIS OF ELEVATIONS

ELEVATIONS SHOWN ON THIS MAP ARE BASED ON A HIGH PRECISION GPS READING ON AN INITIAL CONTROL POINT AS ESTABLISHED WITH LEICS'S "HXGN SMARTNET (NAVD88)



This map being filed in accordance with Section 8762(b) (1) (2) (3) of the Professional Land Surveyors' Act.

ALL DISTANCES AND ELEVATIONS SHOWN ARE IN FEET AND DECIMALS THEREOF.

FD. 1/2" IRON PIN
PER (C) (D)

SUNSET DR.

GLEN UNA DR.

N89°32'W 4.88' (D)

COUNTY SURVEYOR'S STATEMENT

This map has been examined in accordance with Section 8766 of the Professional Land Surveyors' Act this 2nd day of November 2020.

AUGUST HANKS, COUNTY SURVEYOR
Professional Land Surveyor No 9274

By: Jeremy Koenig
JEREMY KOENIG, DEPUTY COUNTY SURVEYOR
Professional Land Surveyor No 9394



APN 510-26-087
O'BRIEN
DOC. #21658276

FD. MAG NAIL, REPLACED
WITH 8" SPIKE AND
TAG, "LS 5418"

FD. 3/4" IP (PER (H))
TAG ILLEGIBLE

86.28'

(N89°54'W 309.35' (A) (H))
S89°51'47"E 309.26'

10' WIDE RIGHT OF WAY PER (E)

222.98'
(223.03' (E)) 10' WIDE RIGHT OF WAY PER (H)

FD. R/R SPIKE PER (A) (C) (D)

RECORDER'S STATEMENT

Filed this 5th day of November 2020, at 12:28 PM in Book 934 of Maps at Page 16, at the request of Robert Craig.

REGINA ALCOMENDRAS

File: 24690744
Fee: \$ 83.00

By Deputy Gina Hopkins

APN 510-26-006
EMAMI
DOC. #24203217

APN 510-26-007
VUPPALA & JALADI
DOC. #24244049

S0°28'00"W 148.96' (158.73' (E))

3' WIDE UTILITY EASEMENT PER (F)

(223.03' (E))
222.99'

N89°55'00"E (B) (G)

APN 510-26-041
KU
DOC. #13349100

N0°28'00"E 148.11' (158.73' (E))

GLEN UNA DR.

128.05' (D)
127.99' (128.07' (C))
S0°28'00"W (A) (B) (C) (D)

FD. PK NAIL PER (C)

FD. 1/2" IP PER (B) (C)

FD. 1/2" IRON BAR
1/2" IP, PER (B) (C)

REFERENCE DOCUMENTS

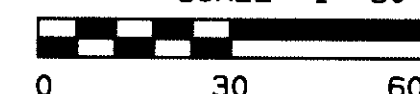
- (A) RECORD OF SURVEY, 87-M-32
- (B) RECORD OF SURVEY, 89-M-20
- (C) RECORD OF SURVEY, 403-M-26
- (D) RECORD OF SURVEY, 751-M-33
- (E) DEED DOC. #24244049
- (F) DEED DOC. #24203217
- (G) DEED DOC. #13349100
- (H) DEED DOC. #21658276

SURVEYOR'S STATEMENT

This map correctly represents a survey made by me or under my direction in conformance with the requirements of the Professional Land Surveyors' Act at the request of Kalyani Jaladi in November, 2019.

ROBERT J. CRAIG
Professional Land Surveyor No. 5418

SCALE 1"=30'

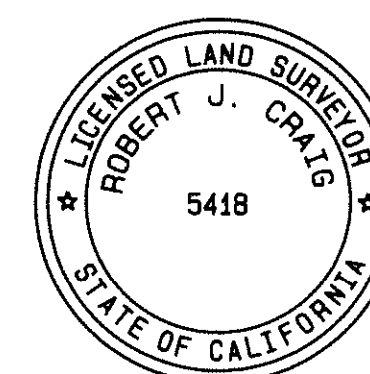


LEGEND

- INDICATES RECORD BOUNDARY LINE
- INDICATES BOUNDARY LINE OF OTHERS
- - - - - INDICATES CENTER LINE OF RIGHT OF WAY
- INDICATES MONUMENT FOUND, AS NOTED
- INDICATES SET 1/2"x36" STEEL ROD, CAPPED "LS 5418", UNLESS NOTED OTHERWISE

BASIS OF BEARINGS

THE BEARINGS SHOWN ON THIS MAP ARE BASED ON THE CENTER LINE OF GLEN UNA DRIVE AS FOUND MONUMENTED AND RECORDED AS NORTH 0°28'00"EAST ON THE RECORD OF SURVEY FILED IN BOOK 403 OF MAPS, PAGE 26, SANTA CLARA COUNTY RECORDS.



ROBERT J. CRAIG

LICENSED LAND SURVEYOR

966 ELSIE MAE DRIVE
BOULDER CREEK, CA 95006
(831) 359-1750
(408) 884-3791
robertjamescraig@gmail.com

RECORD OF SURVEY

LANDS OF VUPPALA & JALADI
AS RECORDED IN DOC. # 24244049
OFFICIAL RECORDS OF SANTA CLARA COUNTY

SITUATED IN

LOS GATOS

UNINCORPORATED AREA OF
SANTA CLARA COUNTY, CALIFORNIA

SCALE 1"= 30'	DRAWN RJC	JOB NO. C-19143	SHEET 1
DATE FEB. 6, 2020	APN 510-26-007	INDEX SANTA CLARA CO.	1
FIELD NIB		GRID 125-24-54	OF 1

24690744

934/
16

16

